

October 4, 2024

Item Summary

Consideration of a proposed 7,852 square foot gymnasium on the Mae Richardson Elementary School property. The 5.92-acre project site is located at 200 W. Pine Street in the Civic zoning district within the Transit Oriented Development (TOD) Overlay and identified on the Jackson County Assessor's map as 37S 2W 10AB, Tax Lot 1200.

Owner: School District 6 (Spencer Davenport);
Agent: arkitek design & architecture (Christopher Brown)

Staff Source

Justin Gindlesperger, Community Planner III

Background

The Applicant is requesting Site Plan and Architectural Review approval to construct an approximately 7,852 square foot gymnasium on the Mae Richardson Elementary School property. The development plan includes the proposed gymnasium, a courtyard with outdoor seating between the existing and proposed structures, and landscaping and screening around the proposed gymnasium. The purpose of the Site Plan and Architectural Review application is to assure the proposed site and building design are consistent with the land use and development standards for the City in accordance with CPMC 17.72, Site Plan and Architectural Review.

Project Description:

Site Design:

The Site Plan (Attachment "A-1") depicts the location of the structure along with proposed courtyard, landscaping and stormwater areas. The Landscape Plan (Attachment "A-2") depicts proposed landscape areas around the perimeter of the building and the courtyard. There is ample landscaping to comply with, or exceed, landscape standards in CPMC 17.75.039. The on site stormwater runoff will connect with an existing facility north of the project site that was installed as part of previous on site improvements (see File No. SPAR-21002).

Access/Circulation/Parking:

As shown on the Site Plan (Attachment "A-1"), the property fronts two (2) public streets including N Haskell Street to the west and W Pine Street to the south. Access to the public streets is existing and the proposal does not include changes to access or on-site circulation. The proposed gymnasium does not increase enrollment or staff on-site, and no additional parking is being provided.

Building Design:

As depicted on the Building Plans (Attachment "A-3"), the building incorporates high-quality, long-lasting, durable exterior materials. The design features variation in colors, materials with a masonry wainscot, metal siding and sloped roof that is consistent with the existing building on-site. Maximum building height is approximately 33-feet, below the maximum of 45-feet in the Civic zoning district.

Issues

There are two (2) issues relative to this application, as follows:

1. **Utility Laterals.** As depicted on the Utility Plan (Attachment "A-4"), the proposed utility connections for the new gymnasium cross the property to the north that is owned by the City of Central Point. There are no easements in place for the utilities crossing the property lines. As noted in the Parks and Public Works Staff Report (Attachment "D"), the additional improvements on the City property require the applicant to either enter into an agreement with the City for a land swap in order to contain underground utilities on the applicant's property or complete a property line adjustment relocating the property lines to encompass the utility lateral location.

Comment: Staff recommends Condition of Approval No. 1(a) requiring the applicant to provide recorded documentation to eliminate the encroachment onto City-owned property, including but not limited to, an agreement for property exchange with the City of Central Point or a Lot Line Adjustment.

2. **Frontage Improvements.** The property frontage along West Pine Street lacks improvements, including sidewalks, landscape row and street trees and the proposed development requires frontage improvements along West Pine Street.

Comment: As noted in the Parks and Public Works Staff Report (Attachment "D"), the City and the applicant are working together on the Safe Routes to School program grant, administered by the Oregon Department of Transportation, to design and construct required facilities along West Pine Street. Public Works has agreed to defer frontage improvements until the grant funds are administered and the project can be built. No conditions are recommended to address sidewalk construction since the Applicant and the City are working collaboratively to connect the sidewalk system at this location.

Findings of Fact & Conclusions of Law

The Site Plan and Architectural Review for the proposed gymnasium has been evaluated against the applicable Site Plan and Architectural Review Criteria set forth in CPMC 17.66 and CPMC 17.72 and found to comply as conditioned and as evidenced in the Applicant's Findings of Fact (Attachments "B") and the Planning Department Supplemental Findings (Attachment "C"). In the event there is a conflict between the Applicant's Findings and the Planning Department Findings, the latter shall apply.

Recommended Conditions of Approval

1. Prior to building permit issuance for the proposed structures, the applicant shall satisfy the following conditions of approval:
 - a. Provide documentation to address the encroachment onto City-owned property along the north side of the proposed improvements including one of the following:
 - i. Recorded documentation of agreement for a property exchange between the property owner (School District 6) and the City of Central Point; or
 - ii. A recorded Final Map of Survey for a Lot Line Adjustment for the area of encroachment. An application and approval of a Property Line Adjustment must precede the recording the Final Map of Survey.
 - b. Demonstrate compliance with the Public Works Department Staff Report (Attachment "D"), including but not limited to:
 - i. Submit and receive approval for Civil Improvement Plans demonstrating compliance with Public Works Department Standard Specifications for public works construction that includes, but is not limited to, the proposed utilities and the protection of public infrastructure.
 - ii. Submit and receive approval for a stormwater management plan demonstrating compliance with the MS4 Phase II stormwater quality standards.
 - iii. Submit and receive approval for an erosion and sediment control permit (NPDES-1200C).
 - iv. Pay all System Development Charges and permit fees.
 - c. Demonstrate compliance with Rogue Valley Sewer Services Staff report, including but not limited to obtaining a connection permit and paying all applicable fees.
2. Prior to Public Works Final Inspection, the applicant shall demonstrate compliance with the following:
 - a. Complete public infrastructure and civil improvements per Civil Improvement Plans approved by the Public Works Department. The Engineer-of-Record shall certify that all improvements were constructed per the approved plans.
 - b. Complete stormwater management improvements per the Stormwater Management Plan approved by the Public Works Department. The Engineer-of-Record shall certify that the construction of the drainage system was constructed per the approved plans.

- c. Record an operations and maintenance agreement for all new stormwater quality features.
3. Any modifications to the site layout, including but not limited to stormwater quality treatment facility type and location, shall be subject to review in accordance with CPMC 17.09, Modifications to Approved Plans and Conditions of Approval.

Attachments

Attachment "A-1" – Master Site Plan

Attachment "A-2" – Landscape Plan

Attachment "A-3" – Building Elevations

Attachment "B" – Applicant's Findings, dated 08/02/2024

Attachment "C" – Planning Department Supplemental Findings

Attachment "D" – Public Works Department Staff Report, dated 09/30/2024

Attachment "E" – Fire District No. 3 Staff Report, dated 09/25/2024

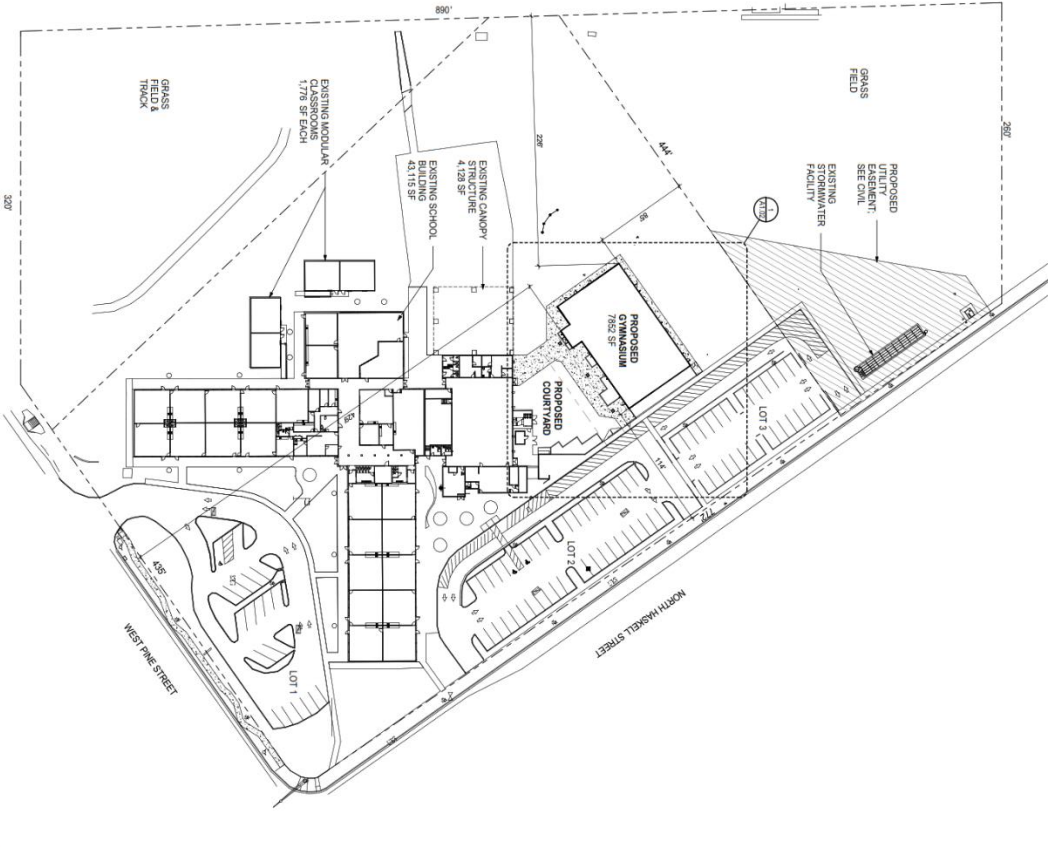
Attachment "F" – Rogue Valley Sewer Services Staff Report, dated 09/18/2024

Action

Consider the Site Plan and Architectural Review application to construct a gymnasium on the property with an existing public elementary school, and 1) Approve; 2) Approve with conditions; or 3) Deny the application.

Recommendation

Approve the Site Plan and Architectural Review application with the conditions per the Staff Report dated October 4, 2024.



1 OVERALL SITE PLAN
1" = 32'



PROJECT INFORMATION
 PROJECT NAME: MRE GYMNASIUM
 ADDRESS: 200 W PINE STREET, CENTRAL POINT, OR 97502
 MAP AND TAXLOT: 372W100B - 1200

CLIENT INFORMATION
 NAME: CENTRAL POINT SCHOOL DISTRICT NO.3
 ADDRESS: 321 W 1ST ST, CENTRAL POINT, OR 97502

NARRATIVE
 THE PROPOSED PROJECT INCLUDES THE CONSTRUCTION OF A NEW 7,852 SF GYMNASIUM BUILDING LOCATED NORTH OF THE EXISTING SCHOOL SITE IMPROVEMENTS INCLUDE SIDEWALKS, A LANDSCAPE PLANTERS, THE NEW AND EXISTING BUILDINGS AND

ZONING INFORMATION

ZONING: CIVIC (C)
 OVERLAY ZONE: TOD OVERLAY
 SETBACKS: MIN. MAX.
 FRONT: 20 FT 0 FT
 SIDE: 20 FT 0 FT
 REAR: 20 FT 0 FT
 CORNER: 5 FT 10 FT
 BUILDING HEIGHT: 48 FT
 MAX LOT COVERAGE: 89%

PARKING CALCULATIONS

VEHICLE PARKING: 86 TOTAL SPACES
 LOT 1: 20 SPACES
 LOT 2: 28 SPACES
 LOT 3: 38 SPACES
 BICYCLE PARKING: 30 SPACES

BUILDING AREA SUMMARY

EXISTING SCHOOL BUILDING: 43,115 SF
 EXISTING MODULAR CLASSROOMS: 3,522 SF
 EXISTING CAMPY STRUCTURE: 4,123 SF
 50,760 SF
 PROPOSED NEW GYMNASIUM: 7,852 SF
 TOTAL EXISTING + PROPOSED: 58,612 SF
 TOTAL INCREASE IN AREA: 15.1%

PLANNING SUMMARY

LOT AREA COVERAGE	AREA	% OF LOT
TOTAL LOT AREA	257,775 SF	100%
MAX ALLOWABLE COVERAGE	219,193 SF	85%
TOTAL STRUCTURES	58,465 SF	22.7%
TOTAL IMPROVED SURFACING	96,220 SF	38.4%
TOTAL LANDSCAPED AREA	100,000 SF	38.9%

arkitek:
 design and llc
 architects llc
 426 a street
 central point, or 97502
 tel: 531.571.9788

SB JAMES
 CONSTRUCTION
 OREGON LICENSE # 97815
 426 a street
 central point, or 97502
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CPSD6
MRE GYMNASIUM
 200 W PINE ST.
 CENTRAL POINT, OR 97502

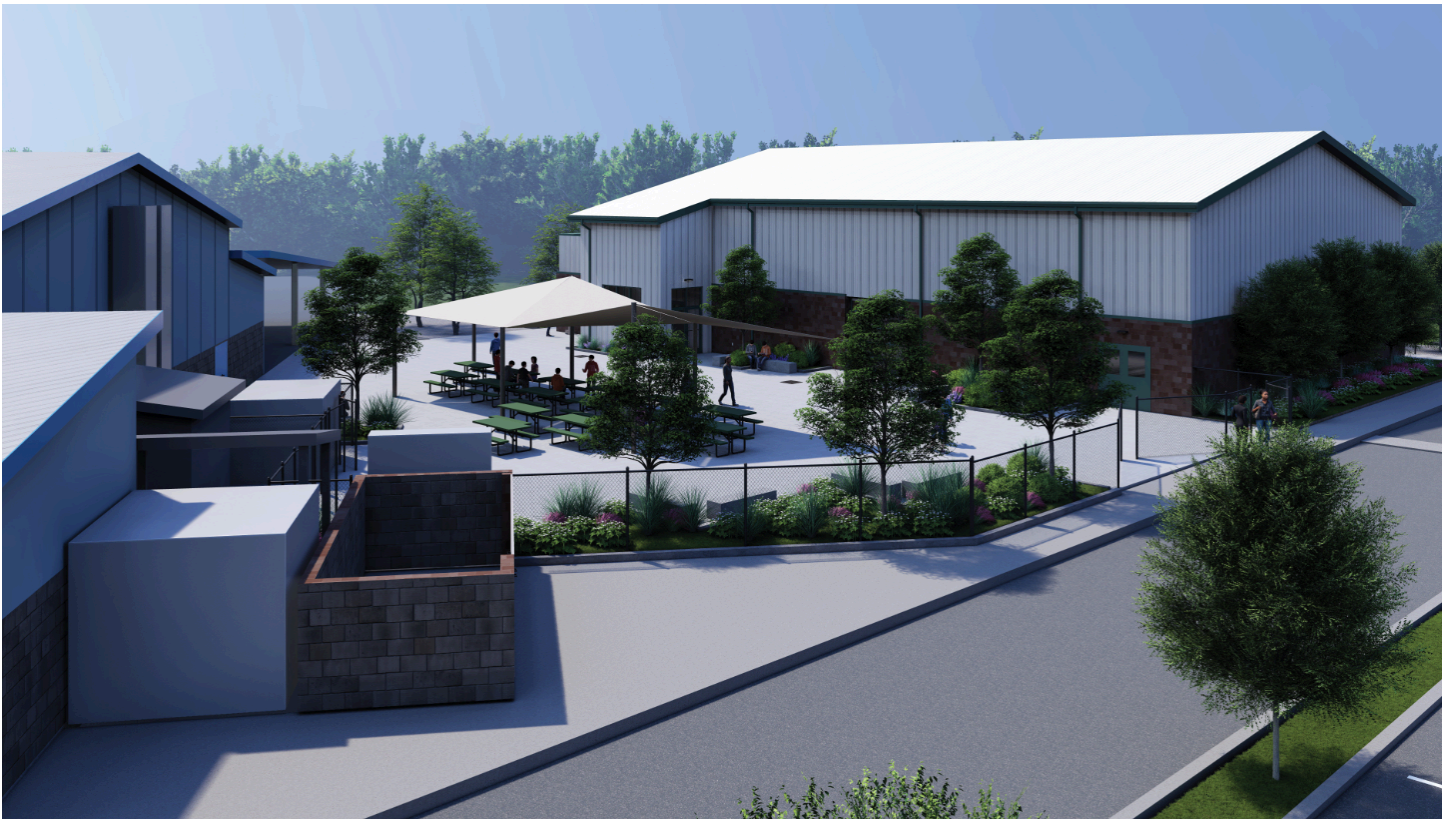
Revision	No.	Description	Date
50% CD	8/21/2024		
23-49			

Project Number: 23-49
 Overall Site Plan

Sheet No. **A1.01**

Central Point School District
Mae Richardson Elementary Gymnasium

200 W Pine St,
Central Point, OR 97502



Planning Application narrative

Aug 30, 2024



MRE Gymnasium

Planning Application Narrative

08/30/2024

PLANNING APPLICATION: MRE Gymnasium

I. Project Information

Pre-Application:	PRE-24006
Subject Site:	200 W Pine St, Central Point, OR 97502
Map & Tax Lot #:	372W10AB - 1200
Zoning:	Civic (C)
Overlay Zone:	TOD overlay
Property Owner:	Central Point School District No.6
Total Acreage:	5.92 Ac (257,875 SF)



II. Project Scope

Mae Richardson Elementary School, located at 200 W Pine St, is anticipating the construction of a new 7,852 sf gymnasium building located north of the existing school. The gym will serve the students of MRE and become a resource for the greater community. Currently, MRE contains just one assembly space within the school, which serves as the cafeteria, the auditorium, and the gymnasium. The purpose of the building is to help alleviate the scheduling and spatial challenges that the school currently faces due to limited space and programming in their singular assembly space. Therefore, the proposed gym building will not be creating any new uses on site, nor will it expand any uses currently on site.

The new gym building will include a multi-court with bleachers, restrooms, and physical education storage space. The restrooms in the proposed gym have interior and exterior access and, when appropriate, may be used during sporting events outside of school hours. A courtyard is proposed between the new and existing building, which intends to provide space for picnic tables to seat approximately 100 students for outdoor lunch. New shrubs and trees will add greenery and shade to a currently bare gravel area.

The applicant seeks approval of a Planning Application in preparation for obtaining a building permit for the proposed development of MRE Gymnasium.

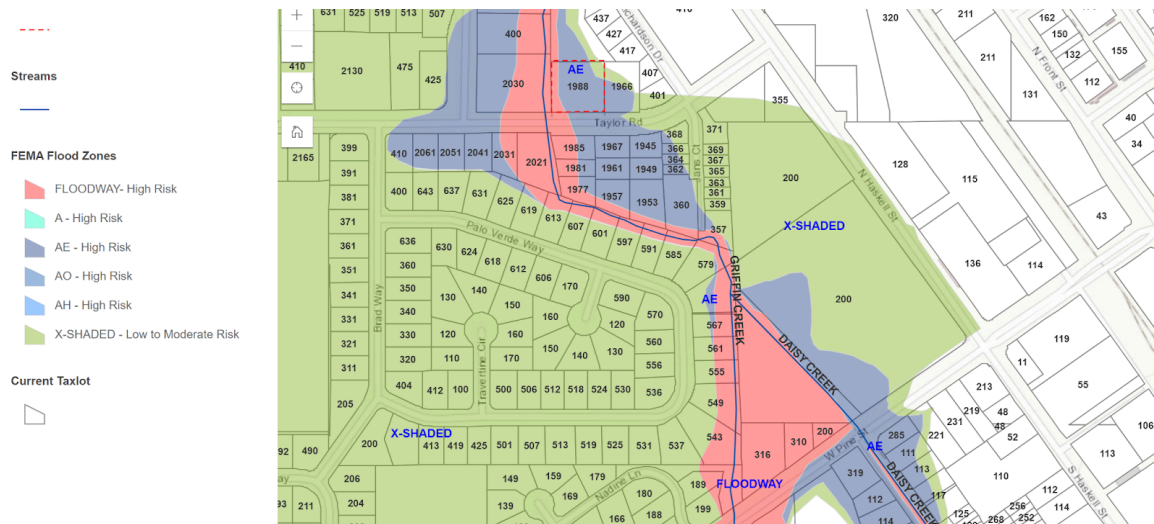
III. Attachments – Evidence of Record

- C1.1 EXISTING CONDITIONS
- C3.0 CIVIL SITE PLAN
- C3.1 SIDEWALK AT W. PINE STREET
- C4.0 DRAINAGE PLAN
- C5.0 SITE UTILITY PLAN
- A1.01 OVERALL SITE PLAN
- A1.02 ARCHITECTURAL SITE PLAN
- A2.01 FLOOR PLAN
- A3.01 EXTERIOR ELEVATIONS
- L.1 LANDSCAPE PLAN

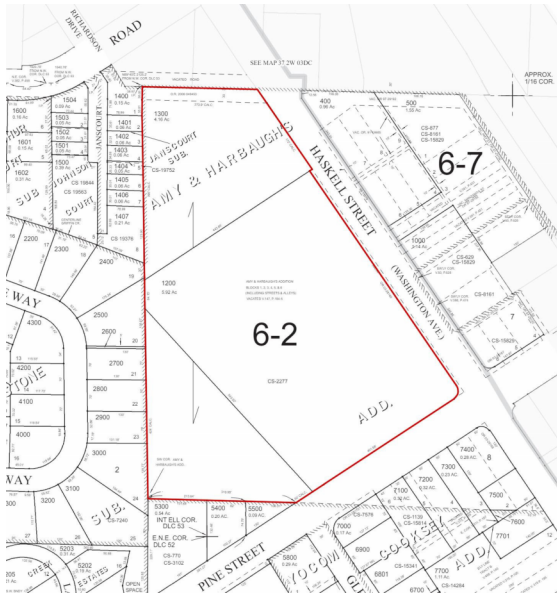
IV. Site Description

The property consists of one existing main building with a footprint of 43,115 sf and two smaller modular buildings located next to it, both at 1,776 sf. Adjacent to the main building, on the west side, there is a 4,128 sf canopy structure for outdoor play. All existing buildings and the project site are within tax lot 1200. There are two points of access to the site, a vehicular access from W Pine Street, reserved for school buses and staff, and an access from N Haskell Street for vehicles, bicycles and pedestrians, used by parents and event participants. The school currently provides 30 bicycle parking spaces at the north side of the building. Two parking lots exist, one along W Pine Street for staff and one along N Haskell Street for the general public. The Haskell parking lot was expanded recently (SPAR-21002) into tax lot 1300 and is equipped with a below ground stormwater detention system. Tax lot 1300, owned by the City of Central Point, is split between the northwest and southwest corners of the property, and is occupied by two large grass fields. A lease agreement exists for the use of the land by CPSD. A recently installed security fence encircles both lots and includes gates at major entrances.

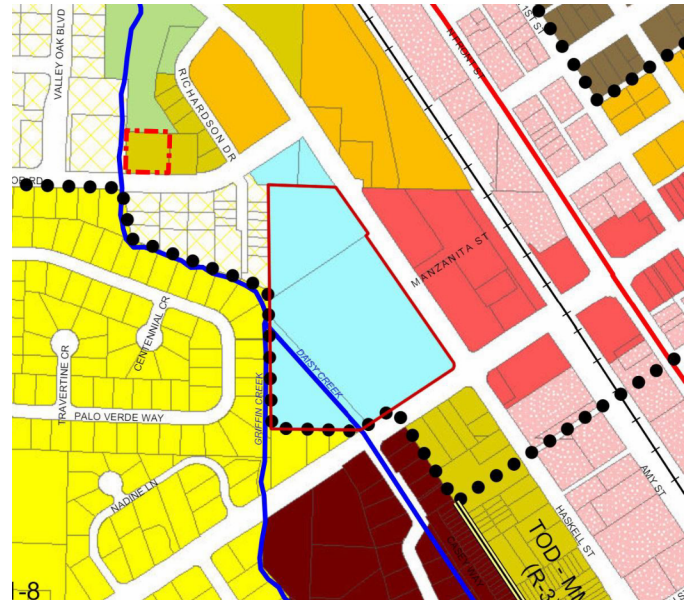
The convergence of Daisy Creek into Griffin Creek occurs on the western boundary of the site, beyond the playing fields. Daisy Creek is piped underneath the lawn between the main building and the running track on the southwest grass field. The southwest track area of the property, between the creeks, is a high-risk flood zone. Most of the property, including the proposed gymnasium, is in the low to moderate risk flood zone.



Underground utilities on W Pine St such as City water, sewer, electricity, and gas currently supply the site and existing school buildings. The proposed development is located in zone Civic (C), within the TOD overlay. A K-12 School is an allowed use within this zone.



Subject parcels



Central point existing zoning map

V. Application Criteria	
<p><u>Applicable Criteria:</u></p>	<p>Compliance Narrative:</p>
<p>17.66.050 Application approval criteria.</p> <p><i>B. Site Plan and Architectural Review. A site plan and architectural review application shall be approved when the approval authority finds that the following criteria are satisfied or can be shown to be inapplicable:</i></p> <ol style="list-style-type: none"> 1. The provisions of Chapter 17.72, Site Plan and Architectural Review, shall be satisfied; and 2. The proposed improvements comply with the approved TOD overlay master plan for the property, if required; and 3. Chapter 17.67, Design Standards--TOD Overlay. 	<ol style="list-style-type: none"> 1. The provisions of chapter 17.72 have been satisfied in the following findings and in the attached design drawings. 2. not applicable 3. The design standards outlined in chapter 17.67 have been incorporated into the proposed development. See the findings below as well as the design drawings attached.

<p>17.72.020 Applicability</p> <p><i>No permit required under Title 15, Buildings and Construction, shall be issued for a major or minor project, as defined in this section, unless an application for site plan and architectural review is submitted and approved, or approved with conditions, as set forth in this chapter.</i></p> <p><i>B. Major Projects. The following are “major projects” for the purposes of the site plan and architectural review process and are subject to Type II procedural requirements as set forth in Chapter 17.05</i></p> <p><i>1. New construction, including private and public projects, that:</i></p> <p><i>a. Includes a new building or building addition of five thousand square feet or more;</i></p>	<p>The proposed development includes a new 7,852 sf gymnasium building addition. The project is therefore a Major Project per Central Point Municipal Code (CPMC) Section 17.72.020(B) and requires a Site Plan and Architectural Review. Per PRE-24006, the project will be subject to Type II review procedures per CPMC 17.05.300. The application form, narrative, mailing labels and fee have been included as part of this complete application.</p>
<p>17.72.040 Approval criteria.</p> <p><i>In approving, conditionally approving, or denying any site plan and architectural review application, the approving authority shall base its decision on compliance with the following criteria:</i></p> <p><i>A. The application is complete in accordance with Section 17.72.030;</i></p> <p><i>B. The proposal complies with all applicable provisions of the zoning district in which it is located including but not limited to setbacks, building height, lot area and dimensions, density and floor area, lot coverage, building design, etc.;</i></p> <p><i>D. The proposal complies with all design and development standards including but not limited to:</i></p>	<p>The drawings listed within section III. Attachments address all listed criteria within 17.72.030.</p> <p>The proposed gymnasium location, size, height, and character are in conformance with all applicable provisions of the Civic district and TOD overlay. See the attached civil, landscape, and architectural drawings for details.</p>

<p>1. Chapter 17.75, Design and Development Standards; 2. Chapter 17.67, Design Standards--TOD Overlay; 3. Chapter 17.64, Off-Street Parking and Loading; and 4. Chapter 17.57, Fences;</p> <p>E. The proposal complies with the city of Central Point Department of Public Works Department Standard Specifications and Uniform Standard Details for Public Works Construction;</p> <p>F. The proposal provides for accessible and sufficient fire fighting facilities necessary to provide for the reasonable safety of life, limb and property, including, but not limited to, suitable gates, access roads and fire lanes so that all buildings on the premises are accessible to fire apparatus.</p>	<p>The proposal complies with the listed code sections. See the findings below and the attached design drawings.</p> <p>The proposal includes a new sidewalk at W Pine Street and new utility tie-ins at N Haskell Street that comply with the City standards. Private and public utility improvements will be developed in detail during the remainder of the project design. Permitting will be addressed at the time of the building permit. See the attached civil design drawings for reference.</p> <p>The proposal includes a new fire hydrant and fire apparatus access to the new development. See the attached design drawings for details.</p>
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<p>VI. Findings of Fact</p>													
<p><u>Applicable Code Section:</u></p>	<p>Compliance Narrative:</p>												
<p>SITE ACCESS AND OFF-STREET PARKING</p> <p>17.64.040 Off-street parking requirements</p> <table border="1" data-bbox="110 1146 776 1304"> <thead> <tr> <th></th> <th>Min.</th> <th>Max</th> </tr> </thead> <tbody> <tr> <td>Schools</td> <td>0</td> <td>1 space per each teacher and staff member plus 3 spaces per classroom</td> </tr> </tbody> </table> <p>17.64.050 Bicycle parking</p> <table border="1" data-bbox="110 1369 776 1493"> <thead> <tr> <th></th> <th>Min.</th> <th>Max</th> </tr> </thead> <tbody> <tr> <td>Schools</td> <td>4 spaces per classroom</td> <td>-</td> </tr> </tbody> </table>		Min.	Max	Schools	0	1 space per each teacher and staff member plus 3 spaces per classroom		Min.	Max	Schools	4 spaces per classroom	-	<p>The existing school has 23 classrooms, 515 students, and approximately 40 staff. 88 parking spaces currently exist on site. The proposed development does not alter the required parking calculations for the use on the site since it does not add classrooms to the site. Therefore, the project is in compliance as the number of spaces does not exceed 109 spaces.</p> <p>The site currently provides a bike rack with 30 spaces, which has been more than sufficient for an elementary school with young kids that do not typically ride their bikes to school. Although the total number of spaces is less than the code requires, the proposed development does not alter the required bicycle parking requirements for the school since it does not add classrooms to the site. The scale and type of development does not trigger the need to bring the number of racks up to compliance.</p> <p>The proposed development will reuse the existing bike rack on site. See sheet A1.02 Architectural Site Plan.</p>
	Min.	Max											
Schools	0	1 space per each teacher and staff member plus 3 spaces per classroom											
	Min.	Max											
Schools	4 spaces per classroom	-											

TOD OVERLAY

17.65.050 Zoning regulations.

- Building Setbacks

	<i>Minimum</i>	<i>Maximum</i>
<i>Front</i>	<i>0 ft</i>	<i>5 ft</i>
<i>Rear</i>	<i>20 ft</i>	<i>0 ft</i>
<i>Side</i>	<i>20 ft</i>	<i>0 ft</i>
<i>Corner</i>	<i>5 ft</i>	<i>10 ft</i>

- *Maximum Building Height : 45 ft*
- *Maximum Lot Coverage: 85%*
- *Minimum Landscaped Area: 15% of site area*

The proposed development meets the TOD overlay zoning regulation requirements as follows :

- The west side of the property is adjacent to a residential zone and the proposed building is located 228 ft from the west boundary.
- Proposed Building Height: 33 ft 10 Inch
- Proposed Lot Coverage: 61.1%
- Proposed Landscaped Area: 38.9%

See sheet A1.01 Overall Site Plan for location and planning summary. See sheet A3.01 for building height.

DESIGN STANDARDS--TOD OVERLAY

17.67.050 Site design standards

F. New Prominent Structures. Key public or civic buildings, such as community centers, churches, schools, libraries, post offices, and museums, should be placed in prominent locations, such as fronting on public squares or where pedestrian street vistas terminate, in order to serve as landmarks and to symbolically reinforce their importance.

L. Lighting

3. On-Site Lighting. Lighting shall be incorporated into the design of a project so that it reinforces the pedestrian environment, provides continuity to an area, and enhances the drama and presence of architectural features. Street lighting should be provided along sidewalks and in medians. Selected street light standards should be appropriately scaled to the pedestrian environment. Adequate illumination should be provided for building entries, corners of buildings, courtyards, plazas and walkways.

K. Landscaping

3. Landscaping Near Buildings. Landscaping shall serve as a screen or buffer to soften the appearance of structures or uses such as parking lots or large blank walls, or to increase the attractiveness of common open spaces.

Existing prominent buildings on site will not be altered as part of the proposed project. The new gymnasium building is proposed to complete the existing MRE campus and will be placed in a location that is visible from N Haskell.

The existing parking lot, school building exterior and canopy structure currently have lighting installed. At the new proposed courtyard, wall-mounted lighting has been incorporated into the building exterior. The proposed building will also have lights installed above each of the exterior doors to encourage a safe pedestrian presence. See the attached A3.01 Exterior Elevations for details.

Shrubs and trees are proposed as part of this project, located in at-grade planting beds at the building foundation and along the existing sidewalk and loading area. See the attached L.1 Landscape Plan for details on location, size, and variety of plantings.

<p><i>4. Service Areas. Service areas, loading zones, waste disposal or storage areas must be fully screened from public view.</i></p> <p><i>b. Acceptable screening includes:</i></p> <ul style="list-style-type: none"> <i>i. A six-foot masonry enclosure, decorative metal fence enclosure, a wood enclosure, or other approved materials complementary to adjacent buildings; or</i> <i>ii. A six-foot solid hedge or other plant material screening as approved.</i> 	<p>The school’s primary service area currently exists at the north end of the building, adjacent to the cafeteria kitchen. A trash enclosure exists, along with an exterior freezer and storage sheds. The proposed project does not propose any changes to the existing service area, however due to the location of the proposed gym building, the storage sheds will be consolidated and relocated to the back side of the existing building. A new landscape planter is also proposed to soften and buffer the service area from N Haskell.</p> <p>The mechanical units at the proposed building will be screened with metal siding on a parapet wall to compliment the building elevation and character.</p>
<p>BUILDING DESIGN</p> <p>17.67.070 Building design standards</p> <p><u>B. Architectural Character</u></p> <p>1. General.</p> <ul style="list-style-type: none"> <i>a. The architectural characteristics of surrounding buildings, including historic buildings, should be considered, especially if a consistent pattern is already established by similar or complementary building articulation, building scale and proportions, setbacks, architectural style, roof forms, building details and fenestration patterns, or materials. In some cases, the existing context is not well defined, or may be undesirable. In such cases, a well-designed new project can establish a pattern or identity from which future development can take its cues.</i> <i>b. Certain buildings, because of their size, purpose or location, should be given prominence and distinct architectural character, reflective of their special function or position. Examples of these special buildings include theaters, hotels, cultural centers, and civic buildings.</i> <p><u>C. Building Entries</u></p> <ul style="list-style-type: none"> <i>a. The orientation of building entries shall:</i> <ul style="list-style-type: none"> <i>i. Orient the primary entrance toward the street rather than the parking lot;</i> <i>ii. Connect the building’s main entrance to the sidewalk with a well-defined pedestrian walkway.</i> <i>d. An exception to any part of the requirements of this section shall be allowed upon finding that:</i> <ul style="list-style-type: none"> <i>ii. The access is to a courtyard or clustered development and identified pedestrian accessways are provided through a parking lot to directly connect the building complex to the most appropriate major pedestrian route(s).</i> <p><u>D. Building Facades.</u></p> <p>1. General.</p>	<p>The proposed building form and massing is developed to complement the existing school building design with respect to its proportion and gable roof. The architectural concepts draw inspiration from local vernacular and industrial aesthetics. The main entrance to the proposed building is located on the side facing the new courtyard, across from the existing school building. Here a vestibule feature extending out of the main building volume marks the access point and gives a clear sense of entry.</p> <p>The main entrance of the proposed building is located on the side facing the new courtyard, across from the existing school building. New walkways along the north west and south west sides of the new building are proposed that connect the parking lot and new courtyard to the building entry points.</p>

<p><i>a. All building frontages greater than forty feet in length shall break any flat, monolithic facade by including discernible architectural elements such as, but not limited to: bay windows, recessed entrances and windows, display windows, cornices, bases, pilasters, columns or other architectural details or articulation combined with changes in materials, so as to provide visual interest and a sense of division, in addition to creating community character and pedestrian scale. The overall design shall recognize that the simple relief provided by window cutouts or sills on an otherwise flat facade, in and of itself, does not meet the requirements of this subsection.</i></p> <p><i>g. All buildings, of any type, constructed within any TOD overlay shall be constructed with exterior building materials and finishes that are of high quality to convey permanence and durability.</i></p> <p><i>h. The exterior walls of all building facades along pedestrian routes, including side or return facades, shall be of suitable durable building materials including the following: stucco, stone, brick, terra cotta, tile, cedar shakes and shingles, beveled or ship-lap or other narrow-course horizontal boards or siding, vertical board-and-batten siding, articulated architectural concrete or concrete masonry units (CMU), or similar materials which are low maintenance, weather-resistant, abrasion-resistant, and easy to clean. Prohibited building materials include the following: plain concrete, plain concrete block, corrugated metal, unarticulated board siding (e.g., T1-11 siding, plain plywood, sheet pressboard), Exterior Insulated Finish Systems (EIFS), and similar quality, nondurable materials.</i></p>	<p>The facade design has been articulated using different materials to create visual interest and pedestrian scale. A 7' tall concrete masonry block veneer wainscot is proposed around the building for durability and to allow students to safely play ball nearby. Metal siding is proposed for the upper half of the façade and at the entire vestibule volume. In addition to elementary school wall ball, junior comets and community teams make use of the fields. Therefore, it is important that there are no breakable or damageable materials used on the exterior of the building. The proposed vestibule feature is proposed with an accent metal siding color to further highlight the entry point. See sheet A3.01 Exterior Elevations for further details. Due to the specific use of the building as a gymnasium that can also serve as assembly space, no windows are proposed so that the space can easily be darkened for presentations using a projector.</p> <p>The exterior building materials that are proposed are masonry and standing seam metal siding. These materials are extremely durable and long-lasting, even in a high-impact site such as an elementary school. The masonry block veneer will have a mix of 2-3 shades of brown to match the existing school facade and create variety along the facade.</p>
<p>17.75.031 <i>General connectivity, circulation and access standards.</i></p> <p><i>C. Driveway and Property Access Standards. Vehicular access to properties shall be located and constructed in accordance with the standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 320.10.30, Driveway and Property Access.</i></p>	<p>Access to the site is existing and circulation was updated as part of a recent application (SPAR-21002) for the new parking lot. The northern access and drive aisle extend onto the adjacent property, owned by the City of Central Point and will serve as the primary access point for the proposed building. The design of the existing parking facilities is in compliance with 17.75.039 Off-Street Parking Design and Development Standards.</p>

FIRE AND LIFE SAFETY

SECTION 503 FIRE APPARATUS ACCESS ROADS

503.1.1 Buildings and facilities. Approved fire apparatus access roads shall be provided for every facility, building or portion of a building hereafter constructed or moved into or within the jurisdiction. The fire apparatus access road shall comply with the requirements of this section and shall extend to within 150 feet (45 720 mm) of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility.

503.2.1 Dimensions. Fire apparatus access roads shall have an unobstructed width of not less than 20 feet (6096 mm), exclusive of shoulders, except for approved security gates in accordance with Section 503.6, and an unobstructed vertical clearance of not less than 13 feet 6 inches (4115 mm).

SECTION 504 ACCESS TO BUILDING OPENINGS AND ROOFS

504.1 Required access. Exterior doors and openings required by this code or the International Building Code shall be maintained readily accessible for emergency access by the fire department. An approved access walkway leading from fire apparatus access roads to exterior openings shall be provided where required by the fire code official.

SECTION 506 KEY BOXES

506.1 Where required. Where access to or within a structure or an area is restricted because of secured openings or where immediate access is necessary for life-saving or fire-fighting purposes, the fire code official is authorized to require a key box to be installed in an approved location. The key box shall be of an approved type listed in accordance with UL 1037, and shall contain keys to gain necessary access as required by the fire code official.

SECTION 507 FIRE PROTECTION WATER SUPPLIES

507.5 Fire hydrant systems.

507.5.1 Where required. Where a portion of the facility or building hereafter constructed or moved into or within the jurisdiction is more than 400 feet (122 m) from a hydrant on a fire apparatus access road, as measured by an approved route around the exterior of the facility or building, on-site fire hydrants and mains shall be provided where required by the fire code official.

The proposed building on site is accessible by fire trucks and emergency vehicles via the fire lane/loading zone at the parking lot along N Haskell Street. This allows for the 150 ft hose line access to the proposed building. Additional 20' clearance has been provided at the proposed courtyard for vehicle accessibility. See the attached C3.0 Civil Site Plan for details.

The fire apparatus access for the building utilizes the existing parking lot and extends access into the proposed courtyard. The width of the new access way is 20 feet and the clearance is unlimited.

A new 6 ft walkway has been proposed connecting the existing parking lot driveway to all the building entry points and the courtyard.

A Key Box is proposed near the riser room of the new building in accordance with UL1037.

An additional fire hydrant is proposed to serve the new building, which is located approximately 250 feet from the farthest corner of the proposed structure. See C5.0 Site Utility Plan for exact location of the proposed fire hydrant.

STORMWATER

**Rogue Valley Stormwater Quality Design Manual
1.5 DESIGN MANUAL APPLICABILITY**

The requirements of this Design Manual apply to Development and Redevelopment, within the limits of any jurisdiction that has adopted the manual.

Table 1.1 Design Manual applicability for Development or Redevelopment.

Location	Impervious Surface area	Requirements
Within city limits	≥ 5,000 sf	Retention and/or Treatment

The proposed development includes additional impervious surfacing on site, including roof area and concrete courtyard area, in excess of 22,100 sf. In 2023, a StormTech advanced drain system was installed underground north of the parking lot improvements. Stormwater from the new impervious surfaces will be routed to the underground ADS which has already been sized for the increased volume of stormwater. See attached sheet C4.0 Drainage Plan for details.

SIDEWALKS

12.04.015 Construction required.

A. Any person building a building or otherwise developing property within the city which is adjacent to a public street or highway shall, as a condition of issuance of a certificate of occupancy for the construction or development, construct sidewalks (including driveway aprons), as defined in this chapter, upon the property. The sidewalk shall be constructed in accordance with the standards of the city for the same, and the city building official is authorized to withhold issuance of a certificate of occupancy until such construction is complete.

The property frontage along N Haskell Street is in compliance with the City standards for sidewalks, including a landscape row, sidewalks and street trees. The frontage along W Pine Street has landscaping and street trees, but no sidewalk currently exists. As part of the proposed project, a sidewalk will be constructed along W Pine Street to bring that section of the frontage into conformance. There are many obstructions along the frontage of W Pine Street that make the construction of a continuous, straight, sidewalk difficult. Therefore the sidewalk will meander as needed to avoid unnecessary relocation of public and private utilities and structures. The design of the sidewalk has been drawn in communication with City staff and will provide a sidewalk as intended in the City’s design standards, while avoiding existing conditions.

**PLANNING DEPARTMENT SUPPLEMENTAL FINDINGS OF FACT
AND CONCLUSIONS OF LAW
File No.: MP-24006**

**Consideration of a Site Plan & Architectural Review
to construct an 8,500 square foot gymnasium, outdoor courtyard and landscape
improvements**

Applicant:) Findings of Fact
Jackson County School District No. 6) and
300 Ash Street) Conclusion of Law
Central Point OR 97502)

**PART 1
INTRODUCTION**

The applicant proposes to construct an approximately 8,500 square foot gymnasium, with an outdoor courtyard and landscaping connecting to the existing Mae Richardson Elementary School.

The site plan and architectural review request is a Major Project, which is processed using Type II application procedures per CPMC 17.72.020(B). Type II procedures set forth in Section 17.05.300 provide the basis for decisions upon standards and criteria in the development code and the comprehensive plan, when appropriate.

The project site is located in the Civic zoning district in the Transit Oriented Development (TOD) Overlay and is subject to the standards and criteria set forth in CPMC 17.66, Application Review Process for the TOD Overlay and CPMC 17.67, Design Standards – TOD Overlay.

Applicable Review Criteria for development on lands within the TOD Overlay are set forth in Chapter 17.66, Application Review Process for the TOD Overlay and include:

- 1. CPMC 17.65.040 and 17.65.050 relating to the TOD Overlay
- 2. CPMC 17.66.030 A(2) and 17.66.050 B – Submittal Requirements
- 3. CPMC 17.67, Design Standards—TOD Overlay
- 4. CPMC 17.72, Site Plan and Architectural Review
- 5. CPMC 17.64, Off-Street Parking and Loading

The Planning Department has reviewed the Applicant’s Findings (Staff Report Attachment “B”) and is applying them as evidence of conformance with the applicable Site Plan and Architectural Review criteria in CPMC 17.66.040 and modified and supplemented by the Planning Department Supplemental Findings set forth herein. If there is a conflict between the Applicant’s Findings and the Planning Department Supplemental Findings, the latter shall apply.

PROJECT BACKGROUND

The property is located at 200 W Pine Street, on the northwest corner of the intersection with N Haskell Street. The 5.92-acre project site is zoned Civic and within the Transit Oriented Development (TOD) Overlay. The Civic zoning district is intended for a variety of uses that are considered public in nature, particularly public schools. While the uses are dependent on the street network for access and visibility, the TOD Overlay provides pedestrian access and proximity to transit.

As shown on the Vicinity Map (Figure 1), the property is currently developed with the Mae Richardson Elementary School. The proposed structure is to provide a separate area for physical education during school hours and will not add to the number of classrooms, staff or students attending the school; therefore, will not adversely affect current traffic, circulation or operations on the site and adjacent public right-of-way.

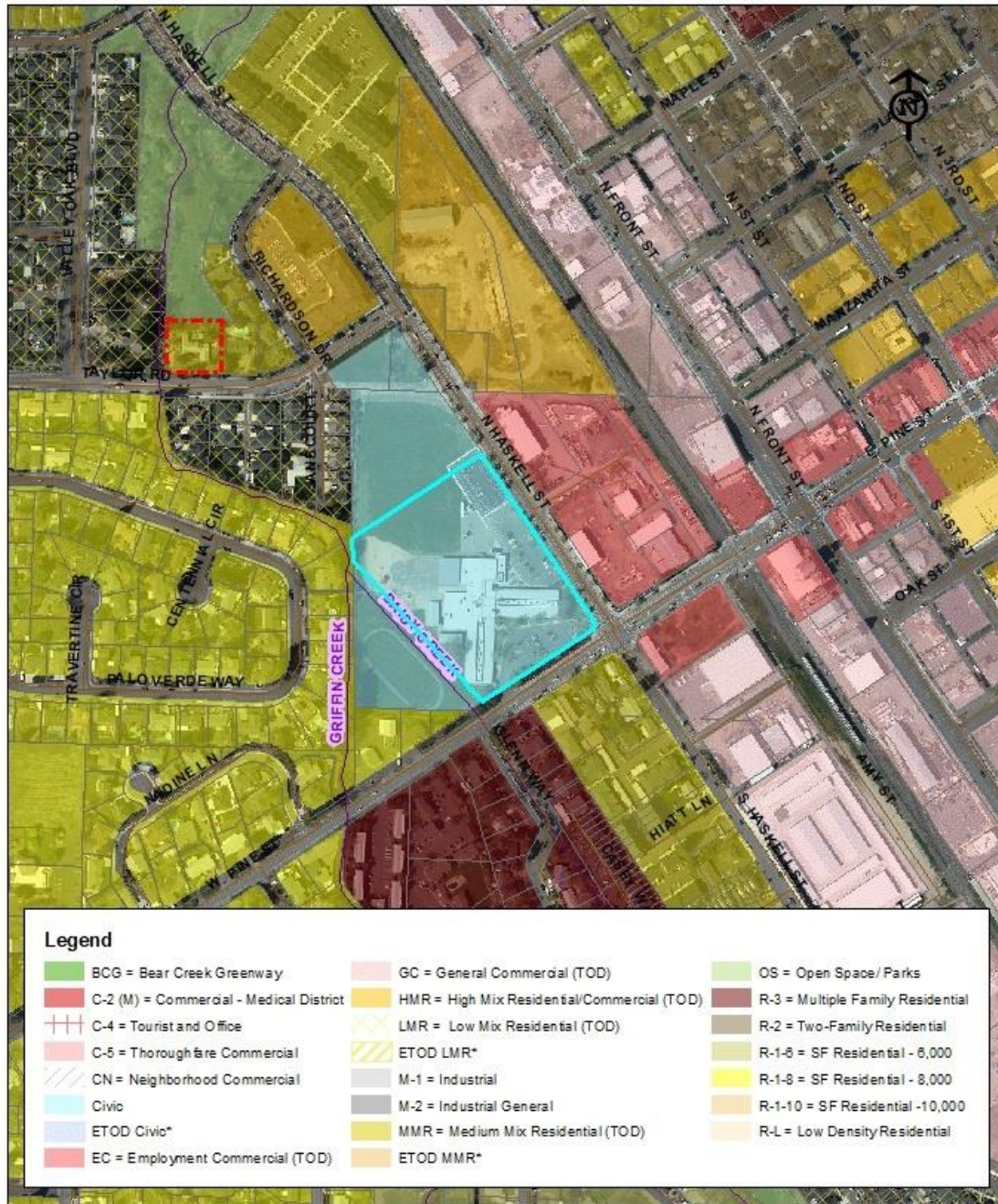
The property owner, Jackson County School District 6, is working with the City of Central Point on an application to the Safe Routes to School program, administered by Oregon Department of Transportation. The application is focused on frontage improvements along W. Pine Street, along the south property boundary of the project site. The program will determine the design of frontage improvements, including sidewalks, that were initially required as part of the current site plan and architectural review application. Once the design is completed through the Safe Routes to School application, the improvements will be installed.

Figure 1. Vicinity Map



Project Location and Zoning Map

**Mae Richardson Gymnasium
SPAR-24006**

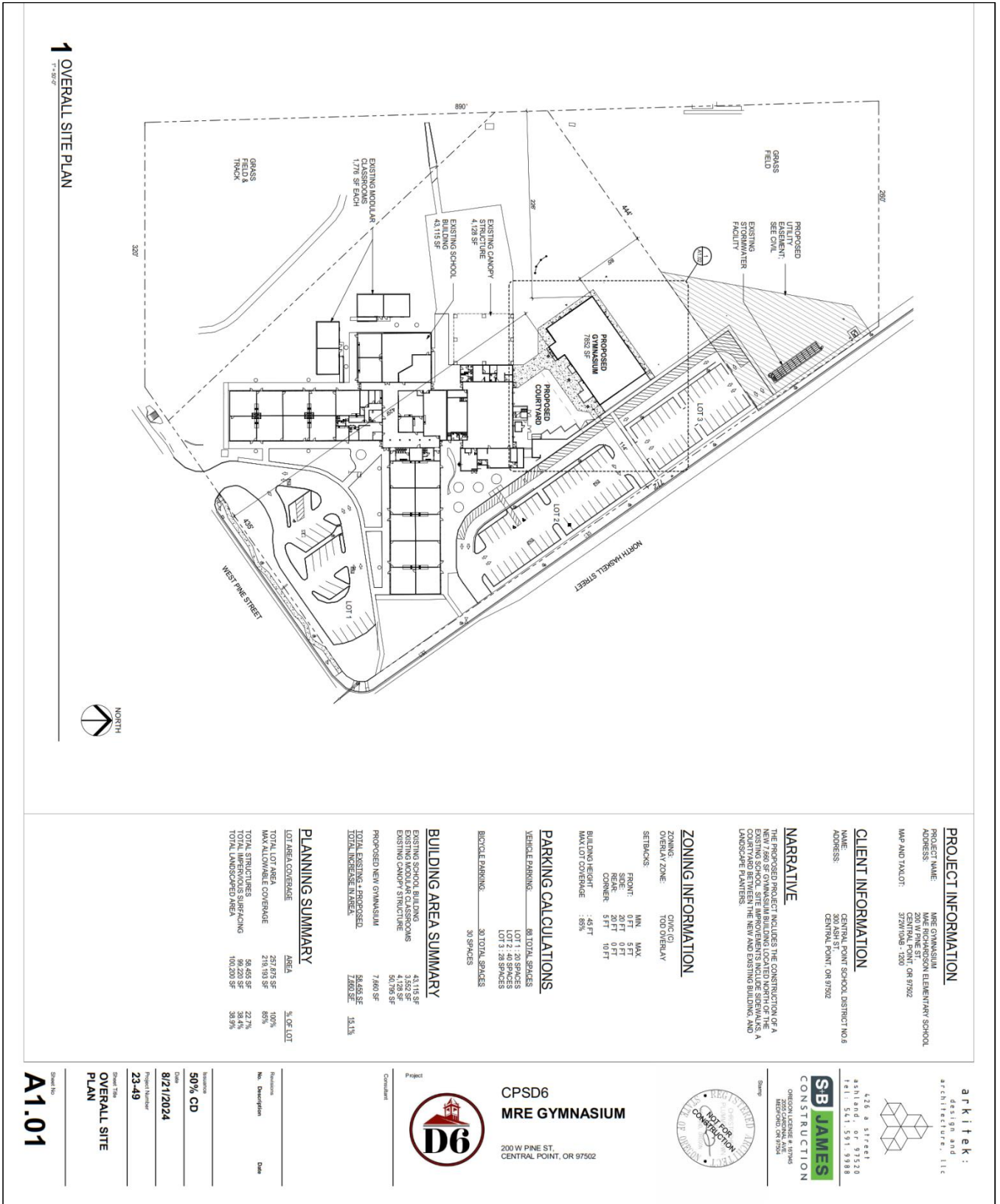


0 200 400 800 Feet

Map Created by: J. Gindlesperger
Date: June 27, 2024

For Illustration Purposes Only

Figure 2. Site Plan



**PART 2 – CHAPTER 17.65
TOD OVERLAY**

The purpose of the Central Point Transit Oriented Development (TOD) overlay is to promote efficient and sustainable land development and the increased use of transit as required by the Oregon Transportation Planning Rule. The sections of CPMC 17.65 applicable to the application are:

17.65.040 Land Use

Four special zone district categories are applied in the Central Point TOD overlay. The characteristics of these zoning districts are summarized in subsections A through D of this section.

A. Residential (TOD).

1. LMR--Low Mix Residential. This is the lowest density residential zone in the district. Single-family detached residences are intended to be the primary housing type; however, attached single-family and lower density multifamily housing types are also allowed and encouraged.
2. MMR--Medium Mix Residential. This medium density residential zone focuses on higher density forms of residential living. The range of housing types includes higher density single-family and a variety of multifamily residences. Low impact commercial activities may also be allowed.
3. HMR--High Mix Residential/Commercial. This is the highest density residential zone intended to be near the center of the TOD district. High density forms of multifamily housing are encouraged along with complementary ground floor commercial uses. Low impact commercial activities may also be allowed. Low density residential uses are not permitted.

B. Employment (TOD).

1. EC--Employment Commercial. Retail, service, and office uses are primarily intended for this district. Activities which are oriented and complementary to pedestrian travel and transit are encouraged. Development is expected to support pedestrian access and transit use. Automobile oriented activities are generally not included in the list of permitted uses. Residential uses above ground floor commercial uses are also consistent with the purpose of this zone.
2. GC--General Commercial. Commercial and industrial uses are primarily intended for this district. Activities which are oriented and complementary to pedestrian travel and transit are encouraged. Residential uses above ground floor commercial uses are also consistent with the purpose of this zone.

- C. C--Civic (TOD). Civic uses such as government offices, schools, and community centers are the primary uses intended in this district. These uses can play an important role in the vitality of the TOD district.
- D. OS--Open Space (TOD). Because the density of development will generally be higher than other areas in the region, providing open space and recreation opportunities for the residents and employees in the TOD overlay becomes very important. This zone is intended to provide a variety of outdoor and recreation amenities.

Finding 17.65.040: *The existing elementary school and proposed gymnasium are located in the Civic zone within the TOD Overlay. The uses are consistent with the zoning district for civic lands within the City.*

Conclusion 17.65.040: *Consistent.*

17.65.050 Zoning Regulations

- A. Permitted Uses. Permitted uses in Table 1 are shown with a “P.” These uses are allowed if they comply with the applicable provisions of this title. They are subject to the same application and review process as other permitted uses identified in this title.
- B. Limited Uses. Limited uses in Table 1 are shown with an “L.” These uses are allowed if they comply with the specific limitations described in this chapter and the applicable provisions of this title. They are subject to the same application and review process as other permitted uses identified in this title.
- C. Conditional Uses. Conditional uses in Table 1 are shown with a “C.” These uses are allowed if they comply with the applicable provisions of this title. They are subject to the same application and review process as other conditional uses identified in this title.

Finding 17.65.050(A-C): *The proposed gymnasium is appurtenant to the elementary school, located in the Civic zone within the TOD Overlay. Community Services, including schools, are listed as permitted Table 1 of this Section. There are no Limited or Conditional Uses considered as part of this application.*

Conclusion 17.65.050(A-C): *Consistent.*

- D. Density. The allowable residential density and employment building floor area are specified in Table 2.
- E. Dimensional Standards. The dimensional standards for lot size, lot dimensions, building setbacks, and building height are specified in Table 2.

Finding 17.65.050(D-E): *A minimum or maximum density is not a requirement for development in the Civic zone. The Civic zoning district does not require minimum lot sizes, average lot sizes or minimum lot width. The proposed development will comply with the*

minimum setbacks and provides landscaping on 39% of the lot area, where a minimum 15% is required.

Conclusion 17.65.050(D-E): *Consistent.*

F. Development Standards.

1. Housing Mix. The required housing mix for the TOD district is shown in Table 2.
2. Accessory Units. Accessory units are allowed as indicated in Table 1. Accessory units shall meet the following standards:

Finding 17.65.050(F)(1-2): *The proposal does not include residential development or accessory units.*

Conclusion 17.65.050(F)(1-2): *Not applicable.*

3. Parking Standards. The off-street parking and loading requirements in Chapter 17.64 shall apply to the TOD overlay.

Finding 17.65.050(F)(3): *CPMC 17.64.040(A) sets forth the minimum and maximum parking requirements for the City in Table 17.64.02. The parking for school uses is based on the number of teachers and classrooms provided on site with zero spaces minimum and 1 space per each teacher and staff member plus 3 spaces per classroom, maximum. As evidenced by the site plan and the Applicant's findings, the proposed development does not increase the number of teachers or classrooms on site and does not include an increase to the existing parking provided on site.*

Conclusion 17.65.070(F)(3): *Not applicable.*

PART 3 – CHAPTER 17.66 APPLICATION REVIEW PROCESS FOR THE TOD OVERLAY

This chapter describes the review procedures to be followed for development proposed within the TOD district and corridor which are identified on the official city zoning map. The sections of CPMC 17.66 applicable to the application are:

CPMC 17.66.030, Application and Review

- A. There are four types of applications which are subject to review within the Central Point TOD overlay. The first of the four types apply in this case.
 1. TOD District or Corridor Master Plan. TOD Overlay Master Plan. Master plans shall be required for:

- a. Development or land division applications which involve two or more acres of land; or
 - b. Modification to a valid master plan approval which involve one or more of the following:
 - i. An increase in dwelling unit density which exceeds five percent of approved density;
 - ii. An increase in commercial gross floor area of ten percent or two thousand square feet, whichever is greater;
 - iii. A change in the type and location of streets, accessways, and parking areas where off-site traffic would be affected; or
 - iv. A modification of a condition imposed as part of the master plan approval.
2. Site Plan and Architectural Review. The provisions of Chapter 17.72, Site Plan and Architectural Review, shall apply to permitted uses and limited uses within the TOD district and corridor. For site plan and architectural review applications involving two or more acres of land, a master plan approval, as provided in this chapter, shall be approved prior to, or concurrently with, a site plan and architectural review application.
 3. Land Division. Partitions and subdivisions shall be reviewed as provided in Title 16, Subdivisions. For a land division application involving two or more acres of land, a master plan approval, as provided in this chapter, shall be approved prior to, or concurrently with, a land division application.
 4. Conditional Use. Conditional uses shall be reviewed as provided in Chapter 17.76, Conditional Use Permits.

Finding CPMC 17.66.030(A): *The current application is for the construction of an approximately 8,500 square foot gymnasium on a 5.92-acre site with an existing elementary school within the Civic zone. The project site was included in the Twin Creeks Master Plan as an area of existing community service use with the potential for future expansion. The proposed gymnasium is consistent with the existing Master Plan and the proposed gymnasium addition does not modify the Master Plan.*

Conclusion CPMC 17.66.030(A): *Not applicable.*

B. Submittal Requirements. A master plan shall include the following elements:

1. Introduction. A written narrative describing:
 - a. Duration of the master plan;
 - b. Site location map;

- c. Land use and minimum and maximum residential densities proposed;
 - d. Identification of other approved master plans within the project area (one hundred feet).
- 2. Site Analysis Map. A map and written narrative of the project area addressing site amenities and challenges on the project site and adjacent lands within one hundred feet of the project site.
 - a. Master Utility Plan. A plan and narrative addressing existing and proposed utilities and utility extensions for water, sanitary sewer, storm water, gas, electricity, and agricultural irrigation.
 - b. Adjacent Land Use Plan. A map identifying adjacent land uses and structures within one hundred feet of the project perimeter and remedies for preservation of livability of adjacent land uses.
- 3. Transportation and Circulation Plan. A transportation impact analysis (TIA) identifying planned transportation facilities, services and networks to be provided concurrently with the development of the master plan and addressing Section 17.67.040, Circulation and access standards.
- 4. Site Plan. A plan and narrative addressing Section 17.67.050, Site design standards.
- 5. Recreation and Open Space Plan. A plan and narrative addressing Section 17.67.060, Public parks and open space design standards.
- 6. Building Design Plan. A written narrative and illustrations addressing Section 17.67.070, Building design standards.
- 7. Transit Plan. A plan identifying proposed, or future, transit facilities (if any).
- 8. Environmental Plan. A plan identifying environmental conditions such as wetlands, flood hazard areas, groundwater conditions, and hazardous sites on and adjacent to the project site.

Finding CPMC 17.66.030(B): See Finding CPMC 17.66.030(A).

Conclusion CPMC 17.66.030(B): Not applicable.

CPMC 17.66.040 Parks and Open Space

Common park and open space shall be provided for all residential development within a TOD overlay as per Section 17.67.060.

Finding CPMC 17.66.040: *The proposed development does not include residential development; therefore, common park and open space are not a requirement.*

Conclusion CPMC 17.66.040: *Not applicable.*

CPMC 17.66.050 Application Approval Criteria

A. TOD Overlay Master Plan. A master plan shall be approved when the approval authority finds that the following criteria are satisfied or can be shown to be inapplicable:

1. Sections 17.65.040 and 17.65.050, relating to the TOD overlay;
2. Chapter 17.67, Design Standards--TOD Overlay;
3. Section 17.65.050, Table 3, TOD Overlay Parking Standards, and Chapter 17.64, Off-Street Parking and Loading;

Finding CPMC 17.66.050(A): *The current application is a Site Plan and Architectural Review. Per Finding 17.66.030(A), a Master Plan is not required.*

Conclusion CPMC 17.66.030(A): *Not applicable.*

B. Site Plan and Architectural Review. A site plan and architectural review application shall be approved when the approval authority finds that the following criteria are satisfied or can be shown to be inapplicable:

1. The provision of Chapter 17.72, Site Plan and Architectural Review, shall be satisfied; and
2. The proposed improvements comply with the approved TOD overlay master plan for the property, if required; and
3. Chapter 17.67, Design Standards – TOD Overlay

Finding CPMC 17.66.030(B): *As evidenced by the findings and conclusions set forth in Applicant's Findings (Staff Report Attachment "B") and Part 4 and Part 5 herein, the proposed gymnasium satisfies the approval criteria for site plan and architectural review.*

Conclusion CPMC 17.66.030(B): *Consistent.*

C. Land Division. A land division application shall be approved when the approval authority finds that the following criteria are satisfied or can be shown to be inapplicable:

Finding CPMC 17.66.050(C): *The current application is a Site Plan and Architectural Review and does not include further land division.*

Conclusion CPMC 17.66.050(C): *Not applicable.*

D. Conditional Use.

Finding CPMC 17.66.050(D): The current application is a Site Plan and Architectural Review and does not include conditional uses as part of the proposed development.

Conclusion CPMC 17.66.050(D): Not applicable.

CPMC 17.66.060 Conditions of approval

The approval authority may apply reasonable conditions of approval to ensure that the applicable standards of this code are satisfied.

Finding CPMC 17.66.060: As evidenced by the findings and conclusions set forth herein, reasonable conditions apply to ensure the standards of this code are satisfied.

Conclusion CPMC 17.66.060: Consistent.

**PART 4 – CHAPTER 17.67
DESIGN STANDARDS – TOD DISTRICT AND TOD CORRIDOR**

The purpose of the Central Point TOD overlay design standards is to complement and support efficient and sustainable land development, to reduce auto reliance and to increase transit use as required by the Oregon Transportation Planning Rule. The sections of CPMC 17.67 applicable to the application are:

CPMC 17.67.040 Circulation and access standards

A. Public Street Standards.

1. Except for specific transportation facilities identified in a TOD overlay master plan, the street dimensional standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 300, Street Construction shall apply for all development located within the TOD overlay which is approved according to the provisions in Section 17.65.020 and Chapter 17.66.
2. Block perimeters shall not exceed two thousand feet measured along the public street right-of-way.
3. Block lengths for public streets shall not exceed six hundred feet between through streets, measured along street right-of-way.
4. Public alleys or major off-street bike/pedestrian pathways, designed as provided in this chapter, may be used to meet the block length or perimeter standards of this section.
5. The standards for block perimeters and lengths shall be modified to the minimum extent necessary based on findings that strict compliance with the standards is not reasonably practicable or appropriate due to:

- a. Topographic constraints;
 - b. Existing development patterns on abutting property which preclude the logical connection of streets or accessways;
 - c. Railroads;
 - d. Traffic safety concerns;
 - e. Functional and operational needs to create a large building; or
 - f. Protection of significant natural resources.
6. All utility lines shall be underground but utility vault access lids may be located in the sidewalk area.
7. Connections shall be provided between new streets in a TOD overlay and existing local and minor collector streets.

Finding 17.67.040(A)(1-7): *The proposal does not include the creation of new streets and will not alter the block length or perimeter block length. The project site has frontage and existing access approaches along North Haskell Street and West Pine Street, existing Minor Arterials (Public Works Standard ST-30). No new utility lines are proposed and service lines to the structure will be located underground.*

Conclusion 17.67.040(A)(1-7): *Consistent.*

8. Pedestrian/Bike Access ways Within Public Street Right-of-Way.
- a. Except for specific access way facilities identified in a TOD overlay master plan, the following access way dimensional standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 300, Street Construction shall apply for any development located within the TOD overlay which is approved according to the provisions in Section 17.65.020 and Chapter 17.66.
9. Public Off-Street Accessways.
- a. Pedestrian accessways and greenways should be provided as needed to supplement pedestrian routes along public streets.
 - b. Major off-street pedestrian accessways shall incorporate all of the following design criteria:
 - i. The applicable standards in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 300, Street Construction;
 - ii. Minimum ten-foot vertical clearance;

- iii. Minimum twenty-foot horizontal barrier clearance for pathway;
 - iv. Asphalt, concrete, gravel, or wood chip surface as approved by the city, with a compacted subgrade;
 - v. Nonskid boardwalks if wetland construction is necessary; and
 - vi. Minimum one hundred square feet of trailhead area at intersections with other pedestrian improvements. A trail map sign shall be provided at this location.
- c. Minor off-street trails shall be a minimum of five feet wide, have a minimum vertical clearance of eight feet, a minimum two-foot horizontal clearance from edge of pathway and be constructed of gravel or wood chips, with a compacted subgrade.

Finding 17.67.040(A)(8-9): *The project site is within the Twin Creeks Master Plan that required pedestrian and bicycle accessways along street frontages. The existing site provides pedestrian and bicycle accessways within the public right-of-way along North Haskell Street and the proposal includes the creation of new pedestrian or bicycle accessways on-site with the use of a courtyard and walkways connecting the proposed gymnasium and the remaining development on campus. The frontage along West Pine Street lacks pedestrian facilities. As noted in the Parks and Public Works Staff Report, dated September 30, 2024, the City and the applicant are working together on the Safe Routes to School program grant, administered by the Oregon Department of Transportation, to complete design of appropriate facilities along West Pine Street. Frontage improvements along West Pine Street are to accommodate completion of the design for the Safe Routes to School program and subsequent construction. No additional requirements or conditions are necessary at this time.*

Conclusion 17.67.040(A)(8-9): *Complies.*

B. Parking Lot Driveways.

1. Parking lot driveways that link public streets and/or private streets with parking stalls shall be designed as private streets, unless one of the following is met:
 - a. The parking lot driveway is less than one hundred feet long;
 - b. The parking lot driveway serves one or two residential units; or
 - c. The parking lot driveway provides direct access to angled parking stalls.
2. The number and width of driveways and curb cuts should be minimized and consolidated when possible.
3. Where possible, parking lots for new development shall be designed to provide vehicular and pedestrian connections to adjacent sites.

4. Large driveways should use distinctive paving patterns.

Finding 17.67.040(B): *As shown on the Site Plan, the proposed development does not include altering the existing driveways nor creating additional parking lot driveways. The existing driveways do not connect to another public or private street and do not provide direct access to parking stalls.*

Conclusion 17.67.040(B): *Not applicable.*

C. On-Site Pedestrian and Bicycle Circulation. Attractive access routes for pedestrian travel should be provided by:

1. Reducing distances between destinations or activity areas such as public sidewalks and building entrances. Where appropriate, develop pedestrian routes through sites and buildings to supplement the public right-of-way;
2. Providing an attractive, convenient pedestrian accessway to building entrances;
3. Bridging across barriers and obstacles such as fragmented pathway systems, wide streets, heavy vehicular traffic, and changes in level by connecting pedestrian pathways with clearly marked crossings and inviting sidewalk design;
4. Integrating signage and lighting system which offers interest and safety for pedestrians;
5. Connecting parking areas and destinations with pedestrian paths identified through use of distinctive paving materials, pavement striping, grade separations, or landscaping.

Finding 17.67.040(C): *As shown on the Site and Landscape Plans, on-site pedestrian circulation is provided by pedestrian accessways that connects the building entrances with the parking areas and sidewalks along North Haskell Street and West Pine Street.*

Conclusion 17.67.040(C): *Consistent.*

17.67.050 Site Design Standards.

The following standards and criteria shall be addressed in the master plan, land division, and/or site plan review process:

A. Adjacent Off-Site Structures and Uses.

1. All off-site structures, including septic systems, drain fields, and domestic wells (within one hundred feet) shall be identified and addressed in the master plan, land division, or site plan process in a manner that preserves and enhances the livability and future development needs of off-site structures and uses consistent with the purpose of the TOD district and as necessary to improve the overall relationship of a development or an individual building to the surrounding context.

2. Specific infrastructure facilities identified on site in the master plan, land division, and/or site plan shall comply with the underground utility standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 400, Storm Water Sewer System and, more specifically, Section 420.10.02, Ground Water Control Plan, in order to safeguard the water resources of adjacent uses.

Finding 17.67.050(A): As noted in the Parks and Public Works Staff Report, dated September 30, 2024, utility laterals must cross an adjacent property, owned by the City of Central Point, in order to provide service to the proposed structure. There is currently a lease agreement for a portion of the adjacent property that provides area for stormwater management from existing development on the project site. As a condition of approval, the applicant is required to submit an application for a property line adjustment or enter into an agreement with the City for a land swap in order to contain underground utilities on the applicant's property.

Conclusion 17.67.050(A): Complies as conditioned.

B. Natural Features.

1. Buildings should be sited to preserve significant trees.
2. Buildings should be sited to avoid or lessen the impact of development on environmentally critical areas such as steep slopes, wetlands, and stream corridors.
3. Whenever possible, wetlands, groves and natural areas should be maintained as public preserves and as open space opportunities in neighborhoods.

Finding 17.67.050(B): The project site is relatively flat and is currently developed with an existing elementary school. The property abuts the Daisy Creek stream corridor to the west; however, the proposed gymnasium is located approximately 300-feet from the creek and is outside the stream corridor and mapped floodplain areas. There are no trees or mapped wetland areas on the site.

Conclusion 17.67.050(B): Not applicable.

C. Topography.

1. Buildings and other site improvements should reflect, rather than obscure, natural topography.
2. Buildings and parking lots should be designed to fit into hillsides, for instance, reducing the need for grading and filling.
3. Where neighboring buildings have responded to similar topographic conditions on their sites in a consistent and positive way, similar treatment for the new structure should be considered.

Finding 17.67.050(C): The project site is relatively flat with minor topographic changes.

Conclusion 17.67.050(C): *Not applicable.*

D. Solar Orientation.

1. The building design, massing and orientation should enhance solar exposure for the project, taking advantage of the climate of Central Point for sun-tempered design.
2. Where possible, the main elevation should be facing within twenty-five degrees due south.
3. In residential developments, the location of rooms should be considered in view of solar exposure, e.g., primary living spaces should be oriented south but a west facing kitchen should be avoided as it may result in summer overheating.
4. Outdoor spaces should be strategically sited for solar access and the cooling summer winds.
5. Shadow impacts, particularly in winter on adjacent buildings and outdoor spaces should be avoided.

Finding 17.67.050(D): *The proposal maximizes solar orientation to the greatest extent possible within the context of the existing street network and adjacent development patterns.*

Conclusion 17.67.050(D): *Consistent.*

E. Existing Buildings on the Site.

1. Where a new building shares the site with an admirable existing building or is a major addition to such a building, the design of the new building should be compatible with the original.
2. New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

Finding 17.67.050(E): *The project site is currently developed with an existing elementary school. As evidenced by the findings and conclusions set forth in Part 4 herein, the proposed structure satisfies the approval criteria for building design standards in the TOD Overlay.*

Conclusion 17.67.050(E): *Consistent.*

- F. New Prominent Structures. Key public or civic buildings, such as community centers, churches, schools, libraries, post offices, and museums, should be placed in prominent locations, such as fronting on public squares or where pedestrian street vistas terminate, in order to serve as landmarks and to symbolically reinforce their importance.

Finding 17.67.050(F): *The proposed development project includes a gymnasium for the existing public elementary school. The proposed project will not alter the existing school building. As evidenced by the findings and conclusions set forth in Part 4 herein, the proposed structure satisfies the approval criteria for building design standards in the TOD Overlay.*

Conclusion 17.67.050(F): *Consistent.*

- G. Views. The massing of individual buildings should be adjusted to preserve important views while benefiting new and existing occupants and surrounding neighborhoods.

Finding 17.67.050(G): *As evidenced by the findings and conclusions for CPMC 17.67.070 set forth herein, the proposed structure satisfies the approval criteria for building design standards for the TOD Overlay.*

Conclusion 17.67.050(G): *Consistent.*

- H. Adjoining Uses and Adjacent Services.

1. When more intensive uses, such as neighborhood commercial or multifamily dwellings, are within or adjacent to existing single-family neighborhoods, care should be taken to minimize the impact of noise, lighting, and traffic on adjacent dwellings.
2. Activity or equipment areas should be strategically located to avoid disturbing adjacent residents.
3. All on-site service areas, loading zones and outdoor storage areas, waste storage, disposal facilities, transformer and utility vaults, and similar activities shall be located in an area not visible from a street or urban space.
4. Screening shall be provided for activities, areas and equipment that will create noise, such as loading and vehicle areas, air conditioning units, heat pumps, exhaust fans, and garbage compactors, to avoid disturbing adjacent residents.
5. Group mailboxes are limited to the number of houses on any given block of development. Only those boxes serving the units may be located on the block. Multiple units of mailboxes may be combined within a centrally located building of four walls that meets the design guidelines for materials, entrance, roof form, windows, etc. The structure must have lighting both inside and out.

Finding 17.67.050(H): *As depicted on the Site Plan (Figure 2) and Landscape Plan (Figure 3), the existing vehicle parking and loading areas located along North Haskell Street and West Pine Street include landscaping between the parking areas and street frontage and within the parking areas. The proposal includes landscaping along the building foundation and along the existing sidewalk and loading area providing addition screening towards North Haskell Street. Proposed utilities are located underground and waste storage will be screened by an enclosure and site landscaping.*

Conclusion 17.67.050(H): *Consistent.*

I. Transitions in Density.

1. Higher density, attached dwelling developments shall minimize impact on adjacent existing lower density, single-family dwelling neighborhoods by adjusting height, massing and materials and/or by providing adequate buffer strips with vegetative screens.
2. Adequate buffer strips with vegetative screens shall be placed to mitigate the impact of higher density development on adjacent lower density development.
3. New residential buildings within fifty feet of existing low density residential development shall be no higher than thirty-five feet and shall be limited to single-family detached or attached units, duplexes, triplexes or fourplexes.
4. New commercial buildings within fifty feet of existing low density residential development shall be no higher than forty-five feet.
5. Dwelling types in a TOD overlay shall be mixed to encourage interaction among people of varying backgrounds and income levels.
6. Zoning changes should occur midblock, not at the street centerline, to ensure that compatible building types face along streets and within neighborhoods. When dissimilar building types face each other across the street because the zoning change is at the street centerline or more infill housing is desired (for instance, duplexes across the street from single dwellings), design shall ensure similarity in massing, setback, and character.
7. Density should be increased incrementally, to buffer existing neighborhoods from incompatible building types or densities. Sequence density, generally, as follows: large lot single dwelling, small lot single dwelling, duplex, townhomes, courtyard multifamily apartments, large multifamily apartments, and mixed use buildings.

Finding 17.67.050(I): *The proposed development is a civic-type development in an area planned and zoned for civic uses, including schools. The project site is currently developed with an existing public elementary school. Existing development in the vicinity of the project site includes light industrial development to the east across North Haskell Street, residential development to the south across West Pine Street, and residential development to the west across the Daisy Creek stream corridor.*

Conclusion 17.67.050(I): *Consistent.*

J. Parking.

1. Parking Lot Location.
 - a. Off-street surface parking lots shall be located to the side or rear of buildings. Parking at midblock or behind buildings is preferred.
 - b. Off-street surface parking lots shall not be located between a front facade of a building and a public street.

- c. If a building adjoins streets or accessways on two or more sides, off-street parking shall be allowed between the building and the pedestrian route in the following order of priority:
 - 1st . Accessways;
 - 2nd. Streets that are non-transit streets.
 - 3rd. Streets that are transit streets.
- d. Parking lots and garages should not be located within twenty feet of a street corner.

Finding 17.67.050(J)(1): *As depicted on the Site Plan (Figure 2), the existing off- street surface parking areas are located to the sides of the existing building and include landscape buffers between the parking lot and the street frontage. The proposal does not include additional or expanded parking lots.*

Conclusion 17.67.050(J)(1): *Not applicable.*

2. Design.

- a. All perimeter and interior landscaped areas must have protective curbs along the edges. Trees must have adequate protection from car doors and bumpers.
- b. A portion of the standard parking space may be landscaped instead of paved. The landscaped area may be up to two feet in front of the space as measured from a line parallel to the direction of the bumper of a vehicle using the space. Landscaping must be ground cover plants. The landscaping does not apply toward any perimeter or interior parking lot landscaping requirements, but does count toward any overall site landscaping requirement.
- c. In order to control dust and mud, all vehicle areas must be paved.
- d. All parking areas must be striped in conformance with the city of Central Point parking dimension standards.
- e. Thoughtful siting of parking and vehicle access should be used to minimize the impact of automobiles on the pedestrian environment, adjacent properties, and pedestrian safety.
- f. Large parking lots should be divided into smaller areas, using, for example, landscaping or special parking patterns.
- g. Parking should be located in lower or upper building levels or in less visible portions of site.

Finding 17.67.050(J)(2): *As shown on the Site Plan (Figure 2) and Landscape Plan (Figure 3), the project site includes existing parking areas with perimeter and interior landscaping. The proposed landscape areas are along the building foundation and within the site away from the parking areas; therefore, the standards in the section do not apply.*

Conclusion 17.67.050(J)(2): *Not applicable.*

3. Additional Standards for LMR, MMR and HMR Zones.

- a. When parking must be located to the side of buildings, parking frontage should be limited to approximately fifty percent of total site frontage.
- b. Where possible, alleys should be used to bring the vehicle access to the back of the site.
- c. For parking structures, see Section 17.67.070(H).

Finding 17.67.050(J)(3): *The project site is located within the Civic zone and is not within the LMR, MMR or HMR zoning districts.*

Conclusion 17.67.050(J)(3): *Not applicable.*

K. Landscaping.

1. Perimeter Screening and Planting.

- a. Landscaped buffers should be used to achieve sufficient screening while still preserving views to allow areas to be watched and guarded by neighbors.
- b. Landscaping should be used to screen and buffer unsightly uses and to separate such incompatible uses as parking areas and waste storage pickup areas.

Finding 17.67.050(K)(1): *Existing landscaping is provided along the street frontages, with the existing parking areas and throughout the site to buffer waste storage areas. Additional landscaping is proposed along the building foundation and between the proposal and the existing parking area.*

Conclusion 17.67.050(K)(1): *Consistent.*

2. Parking Lot Landscaping and Screening.

- a. Parking areas shall be screened with landscaping, fences, walls or a combination thereof.
 - i. Trees shall be planted on the parking area perimeter and shall be spaced at thirty feet on center.

- ii. Live shrubs and ground cover plants shall be planted in the landscaped area.
 - iii. Each tree shall be located in a four-foot by four-foot minimum planting area.
 - iv. Shrub and ground cover beds shall be three feet wide minimum.
 - v. Trees and shrubs must be fully protected from potential damage by vehicles.
- b. Surface parking areas shall provide perimeter parking lot landscaping adjacent to a street that meets one of the following standards:
- i. A five-foot-wide planting strip between the right-of-way and the parking area. The planting strip may be interrupted by pedestrian-accessible and vehicular accessways. Planting strips shall be planted with an evergreen hedge. Hedges shall be no less than thirty-six inches and no more than forty-eight inches in height at maturity. Hedges and other landscaping shall be planted and maintained to afford adequate sight distance for vehicles entering and exiting the parking lot;
 - ii. A solid decorative wall or fence a minimum of thirty-six inches and a maximum of forty-eight inches in height parallel to and not closer than two feet from the edge of right-of-way. The area between the wall or fence and the pedestrian accessway shall be landscaped. The required wall or screening shall be designed to allow for access to the site and sidewalk by pedestrians and shall be constructed and maintained to afford adequate sight distance as described above for vehicles entering and exiting the parking lot;
 - iii. A transparent screen or grille forty-eight inches in height parallel to the edge of right-of-way. A two-foot minimum planting strip shall be located either inside the screen or between the screen and the edge of right-of-way. The planting strip shall be planted with a hedge or other landscaping. Hedges shall be a minimum thirty-six inches and a maximum of forty inches in height at maturity.
- c. Gaps in a building's frontage on a pedestrian street that are adjacent to off-street parking areas and which exceed sixty-five feet in length shall be reduced to no more than sixty-five feet in length through use of a minimum eight-foot-high screen wall. The screen wall shall be solid, grille, mesh or lattice that obscures at least thirty percent of the interior view (e.g., at least thirty percent solid material to seventy percent transparency).
- d. Parking Area Interior Landscaping.

- i. Amount of Landscaping. All surface parking areas with more than ten spaces must provide interior landscaping complying with one or both of the standards stated below.
 - (A) Standard 1. Interior landscaping must be provided at the rate of twenty square feet per stall. At least one tree must be planted for every two hundred square feet of landscaped area. Ground cover plants must completely cover the remainder of the landscaped area.
 - (B) Standard 2. One tree must be provided for every four parking spaces. If surrounded by cement, the tree planting area must have a minimum dimension of four feet. If surrounded by asphalt, the tree planting area must have a minimum dimension of three feet.
- ii. Development Standards for Parking Area Interior Landscaping.
 - (A) All landscaping must comply with applicable standards. Trees and shrubs must be fully protected from potential damage by vehicles.
 - (B) Interior parking area landscaping must be dispersed throughout the parking area. Some trees may be grouped, but the groups must be dispersed.
 - (C) Perimeter landscaping may not substitute for interior landscaping. However, interior landscaping may join perimeter landscaping as long as it extends four feet or more into the parking area from the perimeter landscape line.
 - (D) Parking areas that are thirty feet or less in width may locate their interior landscaping around the edges of the parking area. Interior landscaping placed along an edge is in addition to any required perimeter landscaping.

Finding 17.67.050(K)(2): As depicted on the Landscape Plan (Figure 3), the existing parking areas feature perimeter and interior landscaping. The landscaping is dispersed throughout the parking area and joins perimeter landscaping but does not substitute for interior landscape areas, consistent with this section. The current project for the gymnasium addition does not require additional on-site parking. Therefore, no additional parking area landscaping is required or proposed for the current application.

Conclusion 17.67.050(K)(2): Not applicable.

- 3. Landscaping Near Buildings. Landscaping shall serve as a screen or buffer to soften the appearance of structures or uses such as parking lots or large blank walls, or to increase the attractiveness of common open spaces.

Finding 17.67.050(K)(3): *As depicted on the Landscape Plan, landscape areas along the building foundation and along the existing sidewalks and loading areas provides a mix of shrubs and ground covers along the frontage and sides (where applicable) of the proposed buildings consistent with this requirement.*

Conclusion 17.67.050(K)(3): *Consistent.*

4. Service Areas. Service areas, loading zones, waste disposal or storage areas must be fully screened from public view.
 - a. Prohibited screening includes chain-link fencing with or without slats.
 - b. Acceptable screening includes:
 - i. A six-foot masonry enclosure, decorative metal fence enclosure, a wood enclosure, or other approved materials complementary to adjacent buildings; or
 - ii. A six foot solid hedge or other plant material screening as approved.

Finding 17.67.050(K)(4): *Service areas on-site are currently provided on site for the existing development. The current project proposes to use the existing service areas, currently screened in accordance with this section.*

Conclusion 17.67.050(K)(4): *Not applicable.*

5. Street Trees. Street trees shall be required along both sides of all public streets with a spacing of twenty feet to forty feet on center depending on the mature width of the tree crown, and planted a minimum of two feet from the back of curb. Trees in the right-of-way or sidewalk easements shall be approved according to size, quality, and tree well design, if applicable, and irrigation shall be required. Tree species shall be chosen from the city of Central Point approved street tree list.

Finding 17.67.050(K)(5): *Street trees are currently provided along North Haskell Street at approximately 30-feet on center, as required in this section. Existing landscaping, including trees, is provided along the West Pine Street frontage. As noted in the Parks and Public Works Staff Report, dated September 30, 2024, the property owner and the City are working on a Safe Routes to School program and frontage improvements along West Pine Street are deferred until the design process is completed. As a condition of approval, frontage improvements along West Pine Street, including but not limited to street trees and landscape areas, shall be installed following approval of designs by the Oregon Department of Transportation.*

Conclusion 17.67.050(K)(5): *Not applicable.*

L. Lighting.

1. Minimum Lighting Levels. Minimum lighting levels shall be provided for public safety in all urban spaces open to public circulation.

- a. A minimum average light level of one and two-tenths foot candles is required for urban spaces and sidewalks.
 - b. Metal-halide or lamps with similar color, temperature and efficiency ratings shall be used for general lighting at building exteriors, parking areas, and urban spaces. Sodium-based lamp elements are not allowed.
 - c. Maximum lighting levels should not exceed six foot candles at intersections or one and one-half foot candles in parking areas.
2. Fixture Design in Public Rights-of-Way.
- a. Pedestrian-scale street lighting shall be provided including all pedestrian streets along arterials, major collectors, minor collectors and local streets.
 - b. Pedestrian street lights shall be no taller than twenty feet along arterials and collectors, and sixteen feet along local streets.
3. On-Site Lighting. Lighting shall be incorporated into the design of a project so that it reinforces the pedestrian environment, provides continuity to an area, and enhances the drama and presence of architectural features. Street lighting should be provided along sidewalks and in medians. Selected street light standards should be appropriately scaled to the pedestrian environment. Adequate illumination should be provided for building entries, corners of buildings, courtyards, plazas and walkways.
- a. Accessways through surface parking lots shall be well lighted with fixtures no taller than twenty feet.
 - b. Locate and design exterior lighting of buildings, signs, walkways, parking lots, and other areas to avoid casting light on nearby properties.
 - c. Fixture height and lighting levels shall be commensurate with their intended use and function and shall assure compatibility with neighboring land uses. Baffles shall be incorporated to minimize glare and to focus lighting on its intended area.
 - d. Additional pedestrian-oriented site lighting including step lights, well lights and bollards shall be provided along all courtyard lanes, alleys and off-street bike and pedestrian pathways.
 - e. In addition to lighting streets, sidewalks, and public spaces, additional project lighting is encouraged to highlight and illuminate building entrances, landscaping, parks, and special features.

Finding 17.67.050(L): *The existing parking lot, existing school building and canopy structure include on-site lighting. Additional lighting is proposed for the courtyard area*

and on the proposed gymnasium exterior building walls, including above exterior doorways. On-site lighting is provided for on-site pedestrian movements and connection to the public rights-of-way.

Conclusion 17.67.050(L): Consistent.

M. Signs.

1. The provisions of this section are to be used in conjunction with the city sign regulations in the Central Point Sign Code, Chapter 15.24. The sign requirements in Chapter 15.24 shall govern in the TOD overlay with the exception of the following:
 - a. The types of signs permitted shall be limited only to those signs described in this chapter.
 - b. Decorative exterior murals are allowed and are subject to review and criteria by planning commission or architectural review committee appointed by city council.
 - c. Signs that use images and icons to identify store uses and products are encouraged.
 - d. Projecting signs located to address the pedestrian are encouraged.
2. Sign Requirements. Signs within the TOD overlay shall comply with the standards in Table 17.67.050(1).
 - a. For ground commercial uses in the HMR district.
 - b. For residential uses in the HMR district.
 - c. Scoreboards allowed only as a conditional use within the Civic district.
 - d. Sidewalk A-frame boards (1) within fixed dimensions and not obstructing public right-of-way.
 - e. Temporary commercial banners to promote grand openings, 30 to 60 days per year maximum with planning permit.
3. Sign Materials. Unless otherwise exempt, or authorized by the planning commission, all signs must comply with the following design criteria:
 - a. The base materials for a freestanding sign shall be natural materials including stone, brick, or aggregate.
 - b. Building/sign proportionality as referenced in Table 17.67.050(1).

- c. Sign illumination shall be limited to external illumination to include conventional lighting and neon, if neon is applied to the sign plane area. External illumination is understood to include “back lit” or “halo” lighting. Internally illuminated signs are prohibited except as provided under Table 17.67.050(1) for scoreboards.

4. Prohibited Signs.

- a. Internally illuminated signs;
- b. Roof signs;
- c. Reader boards;
- d. Flashing signs;
- e. Electronic message/image signs on which copy is created through the use of a pattern of lights in a dot matrix configuration, which may be changed intermittently;
- f. Bench signs;
- g. Balloons or streamers.

Finding 17.67.050(M): Signage is not considered or approved as part of this review. Proposed signs are required to apply for a building permit and must comply with the standards in this section.

Conclusion 17.67.050(M): Not applicable.

17.67.060 Public Parks and Open Space Design Standards.

- A. General. Parks and open spaces shall be provided in the TOD overlay and shall be designed to accommodate a variety of activities ranging from active play to passive contemplation for all ages and accessibility.
- B. Parks and Open Space Location.
- C. Parks and Open Space Amount and Size.
- D. Parks and Open Space Design.

Finding 17.67.060: The current application is for the development of a gymnasium on an existing public school property. No additional parks or open space are required.

Conclusion 17.67.060: Not applicable.

17.67.070 Building Design Standards.

- A. General Design Requirements.

1. In recognition of the need to use natural resources carefully and with maximum benefit, the use of “sustainable design” practices is strongly encouraged. In consideration of the climate and ecology of the Central Point area, a variety of strategies can be used to effectively conserve energy and resources:
 - a. Natural ventilation;
 - b. Passive heating and cooling;
 - c. Daylighting;
 - d. Sun-shading devices for solar control;
 - e. Water conservation;
 - f. Appropriate use of building mass and materials; and,
 - g. Careful integration of landscape and buildings. It is recommended that an accepted industry standard such as the U.S. Green Building Council’s LEED™ program be used to identify the most effective strategies. (Information on the LEED™ program can be obtained from the U.S. Green Building Council’s website, www.usgbc.org.)
2. All development along pedestrian routes shall be designed to encourage use by pedestrians by providing a safe, comfortable, and interesting walking environment.
3. Convenient, direct and identifiable building access shall be provided to guide pedestrians between pedestrian streets, accessways, transit facilities and adjacent buildings.
4. Adequate operable windows or roof-lights should be provided for ventilation and summer heat dissipation.

Finding 17.67.070(A): *The proposed building is oriented to provide secure pedestrian access/circulation on the school site. The main entrances are oriented south towards the existing school building with a courtyard providing the on-campus connection. Walkways are provided between parking areas and sidewalks for connectivity to the public right-of-way.*

Conclusion 17.67.070(A): *Consistent.*

B. Architectural Character.

1. General.
 - a. The architectural characteristics of surrounding buildings, including historic buildings, should be considered, especially if a consistent pattern is already established by similar or complementary building articulation, building scale and proportions, setbacks, architectural style, roof forms, building details and fenestration patterns, or materials. In some cases, the

existing context is not well defined, or may be undesirable. In such cases, a well-designed new project can establish a pattern or identity from which future development can take its cues.

- b. Certain buildings, because of their size, purpose or location, should be given prominence and distinct architectural character, reflective of their special function or position. Examples of these special buildings include theaters, hotels, cultural centers, and civic buildings.
- c. Attention should be paid to the following architectural elements:
 - i. Building forms and massing;
 - ii. Building height;
 - iii. Rooflines and parapet features;
 - iv. Special building features (e.g. towers, arcades, entries, canopies, signs and artwork);
 - v. Window size, orientation and detailing;
 - vi. Materials and color; and
 - vii. The building's relationship to the site, climate topography and surrounding buildings.

2. Commercial and High Mix Residential.

- a. Buildings shall be built to the sidewalk edge for a minimum of seventy-five percent of their site's primary street frontage along collector and arterial streets in C, EC, GC, and HMR zones unless the use is primarily residential or the activity that constitutes the request for increased setback is intended to increase pedestrian activity, i.e., pedestrian plaza or outdoor seating area.
- b. Commercial structures and multi-dwellings should be sited and designed to provide a sensitive transition to adjacent lower density residential structures, with consideration for the scale, bulk, height, setback, and architectural character of adjacent single-family dwellings.
- c. In multi-dwelling structures, the plan layout, orientation and window treatment of the building design should not infringe upon the privacy of other adjacent dwellings.

Finding 17.67.070(B): *The architectural characteristics of the proposed building are consistent with the existing school building and the size and location on site will reflect the nature of the use as a public school facility. As depicted on the Site Plan (Figure 2), the building is not located along the sidewalk edge. The gymnasium is incorporated into school campus and is set back from the sidewalks of the drop off areas, consistent with the existing building with existing and proposed landscape will provide a buffer to the public right-of-way.*

Conclusion 17.67.070(B): *Consistent.*

C. Building Entries.

1. General.

- a. The orientation of building entries shall:
 - i. Orient the primary entrance toward the street rather than the parking lot;
 - ii. Connect the building's main entrance to the sidewalk with a well-defined pedestrian walkway.
- b. Building facades over two hundred feet in length facing a street shall provide two or more public building entrances off the street.
- c. All entries fronting a pedestrian accessway shall be sheltered with a minimum four-foot overhang or shelter.
- d. An exception to any part of the requirements of this section shall be allowed upon finding that:
 - i. The slope of the land between the building and the pedestrian street is greater than 1:12 for more than twenty feet and that a more accessible pedestrian route to the building is available from a different side of the building; or
 - ii. The access is to a courtyard or clustered development and identified pedestrian accessways are provided through a parking lot to directly connect the building complex to the most appropriate major pedestrian route(s).

Finding 17.67.070(C)(1): *The main entrances to the building are oriented south towards the existing school building and connected by a courtyard. Pedestrian accessways are provided between the parking lots and public right-of-way to the courtyard and building entrances, with pedestrian access from the parking lot to the building provided by a courtyard. The entrances are covered.*

Conclusion 17.67.070(C)(1): *Consistent.*

2. Commercial and High Mix Residential.

- a. For nonresidential buildings, or nonresidential portions of mixed-use buildings, main building entrances fronting on pedestrian streets shall remain open during normal business hours for that building.
- b. Nonresidential and mixed-use buildings fronting a pedestrian street shall have at least one main building entrance oriented to the pedestrian street.
 - i. Such an entrance shall not require a pedestrian to first pass through a garage, parking lot, or loading area to gain access to the entrance

off or along the pedestrian street, but the entrance may be through a porch, breezeway, arcade, antechamber, portico, outdoor plaza, or similar architectural feature.

- ii. If a building has frontage on more than one street, the building shall provide a main building entrance oriented to at least one of the streets, or a single entrance at the street intersection.
- iii. A building may have more than one main building entrance oriented to a street, and may have other entrances facing off-street parking and loading areas.

Finding 17.67.070(C)(2): *The current application proposes a civic development within the Civic zone. Commercial and high mix residential uses are not included as part of the proposal.*

Conclusion 17.67.070(C)(2): *Not applicable.*

3. Residential.

- a. The main entrance of each primary structure should face the street the site fronts on, except on corner lots, where the main entrance may face either of the streets or be oriented to the corner. For attached dwellings, duplexes, and multi-dwellings that have more than one main entrance, only one main entrance needs to meet this guideline. Entrances that face a shared landscaped courtyard are exempt.
- b. Residential buildings fronting on a street shall have an entrance to the building opening on to the street.
 - i. Single-family detached, attached and row house/townhouse residential units fronting on a pedestrian street shall have separate entries to each dwelling unit directly from the street.
 - ii. Ground floor and upper story dwelling units in a multifamily building fronting a street may share one or more building entries accessible directly from the street, and shall not be accessed through a side yard except for an accessory unit to a single-family detached dwelling.
- c. The main entrances to houses and buildings should be prominent, interesting, and pedestrian-accessible. A porch should be provided to shelter the main entrance and create a transition from outdoor to indoor space.
- d. Generally, single-dwelling porches should be at least eight feet wide and five feet deep and covered by a roof supported by columns or brackets. If the main entrance is to more than one dwelling unit, the covered area

provided by the porch should be at least twelve feet wide and five feet deep.

- e. If the front porch projects out from the building, it should have a roof pitch which matches the roof pitch of the house. If the porch roof is a deck or balcony, it may be flat.
- f. Building elevation changes are encouraged to make a more prominent entrance. The maximum elevation for the entrance should not be more than one-half story in height, or six feet from grade, whichever is less.
- g. The front entrance of a multi-dwelling complex should get architectural emphasis, to create both interest and ease for visual identification.

Finding 17.67.070(C)(3): *The current application proposes a civic development within the Civic zone. Residential uses are not included as part of the proposal.*

Conclusion 17.67.070(C)(3): *Not applicable.*

D. Building Facades.

1. General.

- a. All building frontages greater than forty feet in length shall break any flat, monolithic facade by including discernible architectural elements such as, but not limited to: bay windows, recessed entrances and windows, display windows, cornices, bases, pilasters, columns or other architectural details or articulation combined with changes in materials, so as to provide visual interest and a sense of division, in addition to creating community character and pedestrian scale. The overall design shall recognize that the simple relief provided by window cutouts or sills on an otherwise flat facade, in and of itself, does not meet the requirements of this subsection.
- b. Building designs that result in a street frontage with a uniform and monotonous design style, roofline or facade treatment should be avoided.
- c. Architectural detailing, such as but not limited to, trellis, long overhangs, deep inset windows, should be incorporated to provide sun-shading from the summer sun.
- d. To balance horizontal features on longer facades, vertical building elements shall be emphasized.
- e. The dominant feature of any building frontage that is visible from a pedestrian street or public open space shall be the habitable area with its accompanying windows and doors. Parking lots, garages, and solid wall facades (e.g., warehouses) shall not dominate a pedestrian street frontage.

- f. Developments shall be designed to encourage informal surveillance of streets and other public spaces by maximizing sight lines between the buildings and the street.
- g. All buildings, of any type, constructed within any TOD overlay shall be constructed with exterior building materials and finishes that are of high quality to convey permanence and durability.
- h. The exterior walls of all building facades along pedestrian routes, including side or return facades, shall be of suitable durable building materials including the following: stucco, stone, brick, terra cotta, tile, cedar shakes and shingles, beveled or ship-lap or other narrow-course horizontal boards or siding, vertical board-and-batten siding, articulated architectural concrete or concrete masonry units (CMU), or similar materials which are low maintenance, weather-resistant, abrasion-resistant, and easy to clean. Prohibited building materials include the following: plain concrete, plain concrete block, corrugated metal, unarticulated board siding (e.g., T1-11 siding, plain plywood, sheet pressboard), Exterior Insulated Finish Systems (EIFS), and similar quality, nondurable materials.
- i. All visible building facades along or off a pedestrian route, including side or return facades, are to be treated as part of the main building elevation and articulated in the same manner. Continuity of use of the selected approved materials must be used on these facades.
- j. Ground-floor openings in parking structures, except at points of access, must be covered with grilles, mesh or lattice that obscures at least thirty percent of the interior view (e.g., at least thirty percent solid material to seventy percent transparency).
- k. Appropriately scaled architectural detailing, such as but not limited to moldings or cornices, is encouraged at the roofline of commercial building facades, and where such detailing is present, should be a minimum of at least eight inches wide.
- l. Compatible building designs along a street should be provided through similar massing (building facade, height and width as well as the space between buildings) and frontage setbacks.

Finding 17.67.070(D)(1): *The façade for the commercial development provides architectural detailing and massing consistent with the requirements of this section. Architectural elements include variation in colors and materials, including masonry wainscot and standing seam metal siding, to provide visual interest and a sense of relief along the building facades.*

Conclusion 17.67.070(D)(1): *Consistent.*

2. Commercial and High Mix Residential/Commercial.

- a. In areas adjacent to the transit station, sidewalks in front of buildings shall be covered to at least eight feet from building face to provide protection from sun and rain by use of elements such as: canopies, arcades, or pergolas. Supports for these features shall not impede pedestrian traffic.
- b. Canopies, overhangs or awnings shall be provided over entrances. Awnings at the ground level of buildings are encouraged.
- c. Awnings within the window bays (either above the main glass or the transom light) should not obscure or distract from the appearance of significant architectural features. The color of the awning shall be compatible with its attached building.
- d. Ground floor windows shall meet the following criteria:
 - i. Darkly tinted windows and mirrored windows that block two-way visibility are prohibited as ground floor windows.
 - ii. On the ground floor, buildings shall incorporate large windows, with multi-pane windows and transom lights above encouraged.
 - iii. Ground floor building facades must contain unobscured windows for at least fifty percent of the wall area and seventy-five percent of the wall length within the first ten to twelve feet of wall height.
 - iv. Lower windowsills shall not be more than three feet above grade except where interior floor levels prohibit such placement, in which case the lower windowsill shall not be more than a maximum of four feet above the finished exterior grade.
 - v. Windows shall have vertical emphasis in proportion. Horizontal windows may be created when a combination of vertical windows is grouped together or when a horizontal window is divided by mullions.

Finding 17.67.070(D)(2): *The current application proposes a civic development within the Civic zone. Commercial and high mix residential uses are not included as part of the proposal.*

Conclusion 17.67.070(D)(2): *Not applicable.*

3. Residential.

- a. The facades of single-family attached and detached residences (including duplexes, triplexes, fourplexes, townhouses, and row houses) shall comply with the following standards:

- i. No more than forty percent of the horizontal length of the ground floor front elevation of a single-family detached or attached dwelling shall be an attached garage.
 - ii. When parking is provided in a garage attached to the primary structure and garage doors face the street the front of the garage should not take up more than forty percent of the front facade in plan, and the garage should be set back at least ten feet from the front facade. If a porch is provided, the garage may be set back ten feet from the front of the porch. In addition, garage doors that are part of the street-facing facade of a primary structure should not be more than eighty square feet in area, and there should not be more than one garage door for sixteen feet of building frontage.
 - iii. Residential building elevations facing a pedestrian route shall not consist of undifferentiated blank walls, but shall be articulated with architectural details such as windows, dormers, porch details, balconies or bays.
 - iv. For any exterior wall which is within twenty feet of and facing onto a street or public open space and which has an unobstructed view of that pedestrian street or public open space, at least twenty percent of the ground floor wall area shall be comprised of either display area, windows, or doorways.
 - v. Architectural detailing is encouraged to provide variation among attached units. Architectural detailing includes but is not limited to the following: the use of different exterior siding materials or trim, shutters, different window types or sizes, varying roof lines, balconies or porches, and dormers. The overall design shall recognize that color variation, in and of itself, does not meet the requirements of this subsection.
 - vi. Fences or hedges in a front yard shall not exceed three feet in height. Side yard fencing shall not exceed three feet in height between the front building facade and the street. Fences beyond the front facade of the building in a side yard or back yard and along a street, alley, property line, or bike/pedestrian pathway shall not exceed four feet in height. Fences over four feet in height are not permitted and hedges or vegetative screens in no case shall exceed six feet in height.
- b. The facades of multifamily residences shall comply with the following standards:
- i. Building elevations, including the upper stories, facing a pedestrian route shall not consist of undifferentiated blank walls, but shall be articulated with architectural detailing such as windows, balconies, and dormers.

- ii. For any exterior wall which is within twenty feet of and facing onto a pedestrian street or public open space and which has an unobstructed view of that pedestrian street or public open space, at least twenty percent of the ground floor wall area shall be comprised of either display area, windows, or doorways.
- iii. Arcades or awnings should be provided over sidewalks where ground floor retail or commercial exists, to shelter pedestrians from sun and rain.

Finding 17.67.070(D)(3): *The current application proposes a civic development within the Civic zone. Residential uses are not included as part of the proposal.*

Conclusion 17.67.070(D)(3): *Not applicable.*

E. Roofs.

1. Commercial and High Mix Residential/Commercial.

- a. Roof shapes, surface materials, colors, mechanical equipment and other penthouse functions should be integrated into the total building design. Roof terraces and gardens are encouraged.
- b. When the commercial structure has a flat parapet roof adjacent to pitched roof residential structures, stepped parapets are encouraged so the appearance is a gradual transition of rooflines.

Finding 17.67.070(E)(1): *The current application proposes a civic development within the Civic zone. Commercial and high mix residential uses are not included as part of the proposal.*

Conclusion 17.67.070(E)(1): *Not applicable.*

2. Residential.

- a. Flat roofs with a parapet and cornice are allowed for multifamily residences in all TOD, LMR, MMR and HMR districts, in which the minimum for sloped roofs is 5:12.
- b. Flat roofs with a parapet and cornice are allowed for single-family attached and detached residences (including duplexes, triplexes, fourplexes, townhouses, and row houses) in all TOD residential districts, except the LMR zone.
- c. For all residences with sloped roofs, the roof slope shall be at least 5:12, and no more than 12:12. Eaves shall overhang building walls at a minimum twelve inches deep on all sides (front, back, sides) of a residential structure.

- d. Roof shapes, surface materials, colors, mechanical equipment and other penthouse functions should be integrated into the total building design. Roof terraces and gardens are encouraged.

Finding 17.67.070(E)(2): *The current application proposes a civic development within the Civic zone. Residential uses are not included as part of the proposal.*

Conclusion 17.67.070(E)(2): *Not applicable.*

F. Exterior Building Lighting.

1. Commercial and High Mix Residential/Commercial.

- a. Lighting of a building facade shall be designed to complement the architectural design. Lighting shall not draw inordinate attention to the building.
 - i. Primary lights shall address public sidewalks and/or pedestrian plazas adjacent to the building.
- b. No exterior lighting shall be permitted above the second floor of buildings for the purpose of highlighting the presence of the building if doing so would impact adjacent residential uses.

Finding 17.67.070(F)(1): *The current application proposes a civic development within the Civic zone. Commercial and high mix residential uses are not included as part of the proposal.*

Conclusion 17.67.070(F)(1): *Not applicable.*

2. Residential.

- a. Lighting shall not draw inordinate attention to the building facade.
- b. Porch and entry lights are encouraged on all dwellings to create a safe and inviting pedestrian environment at night.
- c. No exterior lighting exceeding one hundred watts per fixture is permitted in any residential area.

Finding 17.67.070(F)(2): *The current application proposes a civic development within the Civic zone. Residential uses are not included as part of the proposal.*

Conclusion 17.67.070(F)(2): *Not applicable.*

G. Service Zones.

- 1. Buildings and sites shall be organized to group the utilitarian functions away from the public view.

2. Delivery and loading operations, mechanical equipment (HVAC), trash compacting/collection, and other utility and service functions shall be incorporated into the overall design of the building(s) and the landscaping.
3. The visual and acoustic impacts of these functions, along with all wall- or ground-mounted mechanical, electrical and communications equipment, shall be out of view from adjacent properties and public pedestrian streets.
4. Screening materials and landscape screens shall be architecturally compatible with and not inferior to the principal materials of the building.

Finding 17.67.050(G)(2): *The school's primary service area is existing and located on the north side of the existing school building. The site currently features a trash enclosure and the location of the proposed gymnasium will provide additional screening. Additional landscape planters are proposed along the gymnasium and within the courtyard to provide additional screening and buffering.*

Conclusion 17.67.070(G)(2): *Consistent.*

PART 5 – CHAPTER 17.72 SITE PLAN AND ARCHITECTURAL REVIEW

17.72.020 Applicability

No permit required under Title 15, Buildings and Construction, shall be issued for a major or minor project, as defined in this section, unless an application for site plan and architectural review is submitted and approved, or approved with conditions, as set forth in this chapter.

- A. Exempt Projects. Except as provided in subsection (B)(3) of this section the following projects do not require site plan and architectural review:
1. Single-family detached residential structures;
 2. Any multiple-family residential project containing three or less units;
 3. Landscape plans, fences, when not part of a major project;
 4. Storage sheds, patio covers, garages and carports, decks, gazebos, and similar non-occupied structures used in conjunction with residential uses; and
 5. Signs that conform to a previously approved master sign program for the project site.

Exempt projects are required to comply with all applicable development standards of this chapter.

B. Major Projects. The following are “major projects” for the purposes of the site plan and architectural review process and are subject to Type 2 procedural requirements as set forth in Chapter 17.05, Applications and Types of Review Procedures:

1. New construction, including private and public projects, that:
 - a. Includes a new building or building addition of five thousand square feet or more;
 - b. Includes the construction of a parking lot of ten or more parking spaces; or
 - c. Requires one or more variances or conditional use permits and, in the judgment of the director, will have a significant effect upon the aesthetic character of the city or the surrounding area;
2. Any attached residential project that contains four or more units;
3. Any minor project, as defined in subsection C of this section, that the director determines will significantly alter the character, appearance, or use of a building or site.

C. Minor Projects. Except when determined to be an exempt project or a major project pursuant to subsections A and B of this section respectively, the following are defined as “minor projects” for the purposes of site plan and architectural review, and are subject to the Type I procedural requirements of Chapter 17.05, Applications and Types of Review Procedures:

1. New construction, including private and public projects, that involves a new building or building addition of less than five thousand square feet;
2. Signs that meet all applicable standards as set forth in Section 17.75.050, Signage standards;
3. Exterior remodeling within the commercial or industrial zoning districts when not part of a major project;
4. Parking lots less than ten parking spaces;
5. Any project relating to the installation of cabinets containing communications service equipment or facilities owned and operated by a public utility and not subject to Section 17.60.040, Antenna standards;
6. Minor changes to the following:
 - a. Plans that have previously received site plan and architectural review approval;
 - b. Previously approved planned unit developments;

As used in this subsection, the term “minor” means a change that is of little visual significance, does not materially alter the appearance of previously approved improvements, is not proposed for the use of the land in question, and does not alter the character of the structure involved. At the discretion of the director if it is determined that the cumulative effect of multiple minor changes would result in a major change, a new application for site plan and architectural review is required. All minor changes must comply with the development standards of this chapter.

Finding CPMC 17.72.020: *The proposed project includes new construction greater than 5,000 s.f. and qualifies as a Major Project. It is being processed using Type II administrative procedures.*

Conclusion CPMC 17.72.020: *Consistent.*

17.72.030 Information Required

A. General Submission Requirements.

1. Completed and signed application for site plan and architectural review on forms provided by the community development department, including all information and submittals listed on the form; and
2. The application fee prescribed in the city of Central Point planning department fee schedule

Finding CPMC 17.72.030(A): *A completed and signed application was submitted to the Planning Department on September 5, 2024. Included with the application was the prescribed application fee.*

Conclusion 17.72.030(A): *Consistent.*

B. Site Analysis Map.

1. Plan and/or map that includes the entire property and surrounding property to a distance sufficient to determine the project site’s location in the city and the relationship between the project site and adjacent properties;
2. The location and width of all public and private streets, drives, sidewalks, pathways, rights-of-way and easements on the site and adjoining properties;
3. Existing structures, paved areas, drainage ways, ditches, streams, and wetlands;
4. The location, size and species of existing trees on the site that are greater than six inches in diameter at four feet above grade;
5. Flood risk zones mapped on the current flood insurance rate map (FIRM) published by the Federal Emergency Management Agency (FEMA). Include any regulatory floodways and base flood elevation or base depth information as applicable;

6. North arrow, scale and the names and addresses of all persons listed as owners of the project site as listed on the most recently recorded property deed; and
7. Name and address of the project designer, surveyor, engineer and/or planner as applicable.

Finding CPMC 17.72.030(B): *The application includes a Site Plan (Figure 2) that depicts the project site in relation to adjacent properties and development, including the location of public and private streets, sidewalks, and easements. A portion of the subject property is within the Special Flood Hazard Area (SFHA) along the Daisy Creek stream corridor; however, the project site is located outside of the flood and riparian areas. There are no wetlands or other streams on the site. The north arrow, scale and applicable owner, applicant, designer information is included with the application.*

Conclusion 17.72.030(B): *Consistent.*

- C. Proposed Site Plan. The site plan shall be drawn to scale and include the following information as applicable:
 1. The proposed development site including entire property boundary, dimension and gross area;
 2. Features on the site analysis map that are proposed to remain on the site;
 3. The location and width of all public and private streets, drives, sidewalks, pathways, rights-of-way and easements;
 4. The location and dimensions of existing and proposed structures, utilities, parking facilities and other improvements. Include setback dimensions on the site plan and area calculations for existing and proposed buildings on the site plan;
 5. The location, type and area of stormwater treatment facilities, such as bioswales, detention basins, retention basins, etc.;
 6. The location and dimensions of entrances and exits to the site for vehicular, pedestrian, and bicycle access;
 7. The location and dimensions of any proposed parking and vehicle circulation areas including striping, stalls and wheel stops where applicable;
 8. Pedestrian and bicycle circulation areas, including sidewalks, internal pathways, pathway and connections to adjacent properties, bicycle lanes, parking facilities and trails;
 9. Service areas for waste disposal, mail delivery and other loading and delivery areas;

10. Common areas, plazas, outdoor seating, street furniture and other similar improvements, if any;
11. Location, type and height of outdoor lighting;
12. Existing or planned transit stops and/or other public or private transportation facilities; and
13. Location, sizes, types of signs

Finding CPMC 17.72.030(C): *The application includes a Site Plan (Figure 2) that depicts the location of proposed development on the site and includes the existing building, sidewalks, parking areas, landscape areas, utilities, entrances, common areas, waste disposal and delivery areas as required by this section.*

Conclusion 17.72.030(C): *Consistent.*

D. Architectural Drawings. Architectural drawings shall include the following as applicable:

1. Building elevations drawn to scale with dimensions;
2. Building materials, colors and type;
3. Name and contact information for the architect or designer.

Finding CPMC 17.72.030(D): *Architectural drawings included with the application include building elevations, materials, type, colors and the name and contact information of the architect.*

Conclusion 17.72.030(D): *Consistent.*

E. Preliminary Grading Plan. The public works department may require a preliminary grading plan to address utility concerns, natural hazard impacts or other known or suspected geotechnical concerns.

Finding CPMC 17.72.030(E): *A preliminary grading and drainage plan includes proposed site grading and stormwater management areas. There are no natural hazard impacts or suspected geotechnical concerns with the project site*

Conclusion 17.72.030(E): *Consistent.*

F. Landscape Plan.

1. Proposed building envelope, parking lots, and other pavement areas relative to proposed landscape areas;
2. Street trees and planters located within the public right-of-way as required by Chapter 12.36;

3. Site landscape planters and landscaping required in accordance with Section 17.75.039(G), including:
 - a. Street frontage landscaping;
 - b. Adjacent land use/landscaping; and
 - c. Parking lot interior and perimeter landscaping.
4. Planting plan details including the location, size and species of plant materials at the time of planting and crown diameter at fifteen years' maturity;
5. Irrigation plan and details; and
6. Other information as deemed appropriate by the community development director. An arborist's report may require for mature trees.

Finding CPMC 17.72.030(F): *The Landscape Plan (Figure 3) depicts the location of existing parking area, street frontage, and building landscape areas. Proposed planting and landscape areas are depicted along the building foundation and throughout the courtyard area. Planting details include the size, location and species of plant materials to be used on site. Irrigation information and details were not included with the landscape plan. As a condition of approval, the applicant is required to provide an irrigation plan that accompanies the landscape plan.*

Conclusion 17.72.030(F): *Complies as conditioned.*

- G. Mobility Plan. A mobility plan is required to identify the multimodal transportation facilities and services available to provide for the transportation needs of future occupants and visitors to the proposed use. The purpose of the mobility plan is to assure occupants and visitors of the proposed development have access to transportation facilities and services, including the transportation disadvantaged and those who own or lease vehicles for personal or business use. Lack of parking facilities may not be applied as the basis for application denial; however, the plan may be used to require reasonable conditions of approval to provide for multimodal transportation connectivity from the site to nearby facilities when it can be found there is a rational nexus and the condition is proportional to the need generated. A mobility plan shall be a written and illustrated plan that addresses the following:
 1. Location, number and dimensions of accessible parking spaces within two hundred feet of the building entrance as required by the Americans with Disabilities Act;
 2. The location and number of vehicle parking spaces on the project site and within two thousand feet of the project site, including:
 - a. On-street spaces;

- b. Off-street spaces in a public parking lot or parking garage; and
 - c. Shared off-street spaces with another private landowner. Include a copy of the written agreement or easement authorizing the shared parking arrangement;
3. The number of spaces to be equipped with electrical vehicle charging capacity;
 4. The number of spaces with electrical vehicle charging stations and that are equipped with electrical vehicle charging capacity in accordance with Section 17.64.070;
 5. Bicycle parking facility location and dimensions, including the number of spaces provided on the project site;
 6. The location of the nearest transit stop(s), including the route number(s) and service frequency;
 7. Planned public or private shuttle service to the site, if any;
 8. The location and type of existing pedestrian and bicycle travel facilities, such as sidewalks, pedestrian accessways, bike lanes, trails, etc., that connect the project site to the nearest transit stop. Identify any unbuilt or disconnected sections in the existing network; and
 9. For projects that do not include off-street parking in connection with the use, identify the proposed measures to bridge gaps in access to multimodal transportation facilities and services.

Finding 17.72.040(G): *The existing development includes on-site parking for bicycles and vehicles, consistent with the parking requirements in CPMC 17.64 and the proposed development does not increase the required number of parking spaces. No on-street parking spaces are proposed or available along West Pine Street and on-street parking along North Haskell Street is limited during school hours. The nearest transit stop, Route 40, is greater than 1,200 feet from the project site. No electrical vehicle parking or parking area mitigation is required, in accordance with CPMC 17.64.070 and CPMC 17.75.039(I). No additional facilities are proposed or required.*

Conclusion 17.72.040(G): *Consistent.*

- H. Traffic Impact Analysis (TIA). A TIA shall be provided when required pursuant to Section 17.05.900.

Finding CPMC 17.72.030(H): *The proposed development is not estimated to generate additional peak hour trips to the site. Therefore, a TIA is not required.*

Conclusion 17.72.030(H): *Not applicable.*

- I. Deed Restrictions. Copies of existing deed and any proposed deed restrictions or covenants.

Finding CPMC 17.72.030(I): *The proposed development does not include existing or proposed deed restrictions or covenants.*

Conclusion 17.72.030(I): *Not applicable.*

- J. Narrative with Findings of Fact. The applicant shall submit a letter or narrative report documenting compliance with applicable approval criteria in Section 17.72.040.

Finding CPMC 17.72.030(J): *As evidenced by the findings and conclusions for CPMC 17.72.040 set forth herein, the proposed structure satisfies the approval criteria for building design standards for the Site Plan and Architectural Review.*

Conclusion 17.72.030(J): *Consistent.*

- K. Other Information Determined by the Community Development Director. The city may require studies or exhibits prepared by qualified professionals to address specific site features or project impacts (e.g., traffic, noise, environmental features, natural hazards, etc.) as necessary to determine a proposal's conformance with the city's land development regulations in this code.

Finding CPMC 17.72.030(K): *The application for the proposed development was reviewed for completeness and accepted as complete per the notice of completion dated September 13, 2024.*

Conclusion 17.72.030(F): *Consistent.*

17.72.040 Site Plan and Architectural Standards

In approving, conditionally approving or denying any site plan and architectural review application, the approving authority shall base its decision on compliance with the following standards:

- A. The application is complete in accordance with Section 17.72.030;

Finding CPMC 17.72.040(A): *As evidenced by the findings and conclusions for CPMC 17.72.030 set forth herein, the proposed structure satisfies the approval criteria for building design standards for the Site Plan and Architectural Review.*

Conclusion 17.72.040(A): *Consistent.*

- B. The proposal complies with all applicable provisions of the zoning district in which it is located including but not limited to setbacks, building height, lot area and dimensions, density and floor area, lot coverage, building design, etc.;

Finding CPMC 17.72.040(B): *As evidenced by the findings and conclusions for CPMC 17.72.040 set forth herein, the proposed structure satisfies the approval criteria for building design standards for the Site Plan and Architectural Review.*

Conclusion 17.72.040(B): *Consistent.*

- C. The proposal includes any required improvements needed to address nonconforming situations in accordance with Chapter 17.56;

Finding CPMC 17.72.040(C): *The proposal does not include any requirement improvements needed to address nonconforming situations on the subject property.*

Conclusion 17.72.040(C): *Not applicable.*

- D. The proposal complies with all design and development standards including but not limited to:

1. Chapter 17.75, Design and Development Standards;
2. Chapter 17.67, Design Standards--TOD Overlay;
3. Chapter 17.64, Off-Street Parking and Loading; and
4. Chapter 17.57, Fences;

Finding CPMC 17.72.040(D): *The project is located within the TOD Overlay. Design and development standards for projects in the TOD Overlay are found in Chapter 17.67 of the CPMC. As evidenced by the findings and conclusions set forth herein, the proposed structure satisfies the approval criteria for design standards and parking requirements for the Site Plan and Architectural Review. The project will maintain the existing fence between the buildings and the parking/loading area as part of the development.*

Conclusion 17.72.040(D): *Consistent.*

- E. The proposal complies with the city of Central Point Department of Public Works Department Standard Specifications and Uniform Standard Details for Public Works Construction;

Finding 17.72.040(E): *The Parks & Public Works Department reviewed the application for compliance with the Standard Specifications and Uniform Standards Details for Public Works Construction. Frontage improvements along West Pine Street are deferred until the design of the Safe Routes to School program is completed. The review also noted that proposed utility laterals must cross a City of Central Point owned property north of the subject property. As a condition of approval, the applicant is required to submit an application for a lot line adjustment or enter into an agreement with the City for a land swap in order to contain underground utilities on the applicant's property.*

Conclusion 17.72.040(E): *Complies as conditioned.*

- F. The proposal provides for accessible and sufficient fire fighting facilities necessary to provide for the reasonable safety of life, limb and property, including, but not limited to, suitable gates, access roads and fire lanes so that all buildings on the premises are accessible to fire apparatus.

Finding 17.72.040(F): *As evidenced by Fire District #3 email dated 09/25/2024, Fire District #3 staff evaluated the proposal and determined that adequate water supply and access are sufficient.*

Conclusion 17.72.040(F): *Consistent.*

PART 6 CPMC 17.64, OFF-STREET PARKING AND LOADING

CPMC 17.64.020 Applicability

The regulations of this chapter apply to all parking areas in all zones, at all times, whether parking is required by this code or put in for the convenience of property owners or users. All parking shall be developed and maintained to the standards set forth in Section 17.75.039, Off-street parking design and development standards.

Finding 17.64.020: *As depicted on the Site Plan (Figure 2), the project site includes parking areas approved as part of prior site development plans. The proposal does not create additional parking demand on the site and the current application does not propose additional parking areas.*

Conclusion 17.64.020: *Consistent.*

CPMC 17.64.030 Off-street loading

- A. Purpose. The purpose of this section is to provide adequate loading areas for commercial and industrial uses to avoid interference with the operation of adjacent streets.
- B. Applicability. The minimum off-street loading requirements in Table 17.64.01 shall apply in all zoning districts with commercial and industrial uses that will require the receipt or distribution of materials or merchandise by truck or similar vehicle.
- C. Location.
1. Off-street loading facilities shall be located on the same lot or parcel as the structure they are intended to serve.
 2. Off-street loading areas shall not be placed between a building and street frontage unless the following apply:
 - a. The site has frontage along a public street on two or more sides;
 - b. The off-street loading area is not located on the primary building facade.

3. Off-street loading areas shall not be within any required front, side or rear yard setback.
 4. Loading spaces shall not project into any public right-of-way or otherwise interfere with the public use of streets or alleys, sidewalks, or any clear vision triangle.
- D. Loading Berth Dimensions. A loading berth shall not be less than ten feet wide, thirty-five feet long and shall have a minimum height clearance of twelve feet. Where vehicles generally used for loading and unloading exceed these dimensions, the required length of these berths shall be increased.
- E. Landscaping. Off-street loading areas shall be landscaped in accordance with applicable standards in Sections 17.67.050 and 17.75.039 for street frontage, abutting land use perimeter, and parking/loading/maneuvering area requirements for interior and perimeter landscaping.

Finding 17.64.030: *The current application proposes a civic development within the Civic zone. Per the requirements of this section, the public elementary school use does not require loading berth for the use.*

Conclusion 17.64.030: *Not applicable.*

CPMC 17.64.040 Off-street parking requirements

- A. Minimum and Maximum Vehicle Parking Requirements.
1. The minimum and maximum off-street vehicle parking space requirements are set forth in Table 17.64.02 and shall apply to all development unless modified in accordance with subsection C of this section.
 2. Any parking provided to serve a building or use shall include parking spaces accessible to disabled persons in accordance with the Americans with Disabilities Act and subsection D of this section. Accessible spaces provided shall count toward the maximum number of permitted spaces in Table 17.64.02.
 3. Land use categories and their associated uses are subject to the definitions in Section 17.08.410 unless otherwise noted. For purposes of this chapter, if there is a conflict between the definitions in Section 17.08.410 and a defined term elsewhere in the code, Section 17.08.410 takes precedence.

**TABLE 17.64.02 MINIMUM AND MAXIMUM OFF-STREET PARKING STANDARDS
(Abbreviated)**

Land Use Category	Parking Standards (No. spaces per 1,000 square feet of gross floor area unless otherwise noted)	
	Minimum No. Required Spaces	Maximum No. Permitted Spaces
CIVIC USES		
Schools	0 spaces	1 space per each teacher and staff member plus 3 spaces per classroom

B. Calculation.

1. Where the application of the schedule results in a fractional requirement it shall be rounded to the nearest whole number using the common method of rounding.
2. For purposes of this chapter, gross floor area shall not include enclosed or covered areas used for off-street parking or loading, or bicycle facilities.
3. Where uses or activities subject to differing requirements are located in the same structure or on the same site, or are intended to be served by a common facility, the total parking requirement maximum shall be the sum of the requirements for each use or activity computed separately, except as adjusted through the site plan and architectural review process under the provisions of subsection B of this section.
4. Where off-street parking maximums are established on the basis of seats or person capacity, the building code provisions applicable at the time of determination shall be used to define capacity.
5. Where residential use is conducted together with or accessory to other permitted uses, applicable residential requirements shall apply in addition to other nonresidential requirements.

Finding 17.64.040(A-B): *There are currently no minimum parking requirements within the City. As required by Table 17.64.02, developments must still comply with maximum parking standards. Maximum parking standards for an elementary school are based on the number of classrooms, teachers and staff on site. The proposed gymnasium does not increase parking demand on the site and there are currently 88 parking spaces, less than the maximum parking for 23 classrooms and 40 staff members, consistent the requirements of this section.*

Conclusion 17.64.040(A-B): *Consistent.*

- C. Adjustments to Nonresidential Off-Street Vehicle Parking. An applicant may request an increase to the maximum parking standard for a proposed use under subsection A of this

section for review and action by the community development director or planning commission through site plan and architectural review using Type II or Type III procedures in Section 17.05.300 or 17.05.400 as applicable. The applicant's proposal shall consist of a written request and parking analysis prepared by a qualified professional, such as a traffic engineer. At a minimum, the request must assess the average parking demand for the existing and proposed uses on the subject site; opportunities for shared parking with other uses in the vicinity; existing public parking in the vicinity; transportation options existing or planned near the site, such as frequent transit service; and other relevant factors.

Finding 17.64.040(C): *The Applicant is not requesting an adjustment to the required number of spaces. There are 90 parking spaces on site, within the required minimum/maximum number of spaces for the proposed uses.*

Conclusion 17.64.040(C): *Not applicable.*

D. Accessible Parking Requirements. Where parking is provided in an off-street parking lot or garage, ADA-accessible parking shall be provided, constructed, striped, signed and maintained as required by ORS 447.233, and Section 1106 of the latest Oregon Structural Specialty Code as set forth in this section.

1. The minimum number of accessible parking spaces shall be provided for all uses in accordance with the standards in Oregon Structural Specialty Code, Minimum Number of Accessible Parking Spaces. The accessible parking requirements are minimum requirements and are not subject to reductions or exceptions. Where a use demands provision of more ADA-accessible parking spaces than the minimum requirement, parking shall be subject to the parking maximums in Table 17.64.02 and the adjustment provisions in subsection C of this section.

Finding 17.64.040(D): *The site currently provides four (4) ADA-accessible spaces, consistent with ORS 447.233, in parking lots with between 76 and 100 spaces. The current application does not propose additional on-site parking and no additional ADA-accessible spaces are required at this time.*

Conclusion 17.64.040(D): *Not applicable.*

CPMC 17.64.050 Bicycle parking

- A. Purpose. The bicycle parking and storage provisions in this section are intended to provide bicycle parking facilities to accommodate and encourage bicycle travel.
- B. Bicycle Parking Standards. Bicycle parking shall be provided in accordance with Table 17.64.043, Bicycle Parking Requirements.
- C. Exceptions to Bicycle Parking. The approving authority may allow exceptions or reductions to the bicycle parking standards as part of the site plan and architectural review process in connection with the following:

1. Temporary uses such as Christmas tree sales; or
2. Uses that do not generate the need for bicycle parking per a bicycle parking demand analysis that demonstrates and documents justification for the proposed reduction.

D. Bicycle Parking Facility Design. Bicycle parking facility design shall conform to the standards in Section 17.75.039(H).

Finding 17.64.050: *The current application does not increase the required number of bicycle parking spaces on site and no additional bicycle parking is proposed .*

Conclusion 17.64.050: *Not applicable.*

CPMC 17.64.060 Parking demand management

- A. Preferential Parking. New developments with designated employee parking areas shall provide preferential parking for carpools and vanpools. These spaces shall be clearly marked prior to certificate of occupancy of any building permit.
- B. Parking Lot Redevelopment. Portions of existing underused parking lots may be redeveloped to provide bicycle- and transit-oriented facilities, including: bicycle parking, bus stops and pullouts, bus shelters, park and ride stations, and similar facilities.
- C. Shared Parking. The city recognizes the potential benefits of sharing parking between uses, especially when such arrangements take advantage of existing underused parking facilities. In addition to minimizing development cost associated with parking infrastructure, reducing parking lot areas lessens impervious surface area and associated increases in ambient air temperature and water run-off quantity and quality. The city encourages shared parking to serve the needs of multiple uses/developments when possible to provide public health, safety and welfare benefits. To ensure shared parking agreements will provide for parking needs over the long term, new development in the city's employment zones that provide off-street parking via a shared parking agreement with another landowner shall submit a recorded shared parking easement prior to building permit issuance.

Finding 17.64.060: *The parking area for the site is existing and the application does not include preferential, parking redevelopment or shared parking. The provisions of this section do not apply.*

Conclusion 17.64.060: *Not applicable.*

CPMC 17.64.070 Electrical vehicle charging

- A. Capacity. Pursuant to ORS 455.417 and OAR 660-012-0410, electrical conduit shall be installed for no less than forty percent of all vehicle parking spaces provided for new

multifamily residential buildings with five or more units; and new mixed-use buildings with privately owned commercial space and five or more dwelling units.

1. The conduit system must be, at a minimum, capable of supporting the installation of electrical wiring for the future installation of electric vehicle charging stations rated “Level 2” (40 amp/3.3 – 6.6 kW) or larger.
2. Any conduit installed for future electric vehicle charging stations must be labeled “for electric vehicle charging readiness future EV charging stations.” Both ends of the conduit must be labeled for the environment it is located in.

Finding 17.64.070: *The proposed development is for a civic use in a civic zoning district. The development plan and application do not include residential units or mixed use development.*

Conclusion 17.64.070: *Not applicable.*

PART 7 SUMMARY CONCLUSION

As evidenced in Applicant’s Findings and the Planning Department Supplemental Findings, the proposed site plan and architectural review application for the public school gymnasium along West Pine Street is, as conditioned in the Staff Report dated October 4, 2024, in compliance with the applicable criteria set forth in Title 17 of the Central Point Municipal Code.



PUBLIC WORKS STAFF REPORT

September 30, 2024

AGENDA ITEM: Site Plan and Architectural Review (SPAR-24006) application to construct a 7,582-square-foot gymnasium.

Agent: Arkitek, Inc.

Applicant: School District 6.

Traffic:

A traffic impact analysis was not required for this facility as the size of the school does not change in terms of the total number of students. The proposed building separates uses and does not add capacity.

Existing Infrastructure:

Water: 8-inch waterlines surround the subject site.

Streets: Public Streets surround the site.

Stormwater: There is a 24-inch storm drain in West Pine and N. Haskell.

SW Quality: There are currently no stormwater quality features on site.

Background:

This application is for a new gymnasium.

Issues:

There are two issues associated with the proposed gymnasium project as follows:

1. The applicant must cross existing City-owned property to extend a sewer and water laterals to the site. There are no easements in place to allow the extension of needed utility services for the new gymnasium that would grant legal right to cross property boundaries. Public Works has coordinated with the School District on remedying this by transferring ownership of the subject property to the school district by either: 1) Adjusting the property line; or, 2) coordinating with the City on a land swap.

There are currently no sidewalks along the frontage of West Pine Street. The City is working with the School District on a Safe Routes to School grant application to determine the best improvement for this area. It will delay that improvement until an engineering solution is agreed upon. The City would typically require that the Applicant enter into a Deferred Improvement Agreement (DIA). However, the grant is being pursued as a collaborative effort between the City and School District. Ultimately, this will fund the required sidewalk improvements design and construction. To avoid unnecessary cost and delay, Public Works is not recommending a condition to record a DIA.

Conditions of Approval:

Prior to building permit issuance and the start of construction, the following conditions shall be satisfied:

1. Stormwater Management

- a. NPDES Stormwater Management Plan - The applicant shall submit to Public Works for review and approval a stormwater management plan in accordance with the Rogue Valley Stormwater Quality Design Manual, which requires stormwater quantity and quality treatment of all proposed impervious surfaces proposed as part of the partition.
 - b. Civil Improvement Plan Review – The applicant shall submit civil improvement plans for stormwater infrastructure construction, including but not limited to storm drain lines, detention facilities, and the proposed outfall.
 - c. Erosion and Sediment Control – Construction of required improvements will disturb more than one acre. The applicant shall obtain an erosion and sediment control permit (NPDES 1200-C) from the Department of Environmental Quality (DEQ) and provide a copy to the Public Works Department.
2. **Lateral Connections** – The School District shall work with the City of Central Point to either proceed with a property of line adjustment so that services can be extended to the site or property exchanges for the area around Mae Richardson, which the City currently owns.

Prior to Public Works final inspection, the following conditions shall be satisfied:

1. **PW Standards and Specifications** – Applicant shall demonstrate that all Public Works infrastructure construction complies with the Standards Specifications and Uniform Details for Construction. Installation of civil improvement, including but not limited to landscaping, irrigation and sidewalks, shall be installed per approved plans.
2. **Stormwater Quality Operations & Maintenance** – The Applicant shall record an Operations and Maintenance Agreement for all new stormwater quality features and provide a copy of the Public Works Department's recorded document.

Justin P. Gindlesperger

From: Mark Northrop <MarkN@jcf3.com>
Sent: Wednesday, September 25, 2024 1:28 PM
To: Justin P. Gindlesperger
Subject: Re: Request for Comments - SPAR-24006

[EXTERNAL EMAIL] DO NOT CLICK links or attachments unless you recognize the sender and know the content is safe.



Justin:

I have no comments. Access and water supply is covered under the proposed plan.



DFM Mark Northrop, IAAI, CFI

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Markn@jcf3.com

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Cell 541.660.7689

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Together We're Better

From: Justin P. Gindlesperger <Justin.Gindlesperger@centralpointoregon.gov>
Sent: Monday, September 16, 2024 11:07 AM
To: Matt Samitore <Matt.Samitore@centralpointoregon.gov>; Stephanie Powers <stephanie.powers@centralpointoregon.gov>; Mark Northrop <MarkN@jcf3.com>; Mike Ono <Mike.Ono@centralpointoregon.gov>; Greg Graves <Greg.Graves@centralpointoregon.gov>; Mark Kimmelshue



ROGUE VALLEY
SEWER SERVICES
CLEAN WATER - HEALTHY COMMUNITIES

September 16, 2024

City of Central Point Planning Department
155 South Second Street
Central Point, Oregon 97502

Re: PRE 24006 – Mae Richardson Elementary, Map 37 2w 10AB 1200

Our records show an existing 8 inch sewer service extended to the property from the existing main along W Pine Street. Our records are unclear as to the location of the service once it enters the property. Sewer service to the proposed development can be had by connecting to the existing private sewer system on the property. The permit for this connection will be issued by RVSS upon payment of related development fees.

Rogue Valley Sewer Services requests that approval of this development be subject to the following conditions:

1. The applicant must obtain a connection permit from RVSS and pay all related fees.
2. The applicant must submit architectural plans to RVSS for the calculation of related SDC fees.

Feel free to call me if you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "Nick Bakke".

Nicholas R Bakke, PE
District Engineer